

# **LINDEN STREET CORRIDOR MASTER PLAN**

## **Town of Wellesley, Massachusetts**



photo taken by: Alex MacLean - Landslides

Submitted to:  
**Town of Wellesley, Planning Board  
and  
Linden Street Project Committee**

Prepared by:  
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**December 2002**



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June 16, 2003

Lance Hansen  
Planning Department  
Town of Wellesley  
525 Washington Street  
Wellesley, MA 02482

**RE: Final Planning Report  
Linden Street Corridor Master Plan**

Dear Mr. Hansen: *Lance*

We are delighted to submit twenty copies of our final plan for the Linden Street Corridor. This plan was developed with a high level of interaction with community residents, business owners, and town officials. You are to be commended for conducting such an interactive and inclusive process. Although consensus was not reached on all the recommendations contained in the plan, there were a number of action items on which participants did concur.

Improving the pedestrian environment by rebuilding sidewalks, developing safe street crossings, and providing greater amenities will be a significant enhancement. Improving aesthetics by enhancing plantings on the street and in parking lots will be a welcome improvement. Improving the environment for business by streamlining review processes, having more flexible sign guidelines, and promoting a broader mix of uses, will enhance an already vibrant commercial area.

We trust that this planning process is just the beginning of a dialog to guide the future growth and development of Linden Street and we have thoroughly enjoyed our work with the Town and the community.

Sincerely,  
**DUFRESNE HENRY**

  
Ted Brovitz  
Project Manager/Planner

  
Peter F. Jackson  
Landscape Architect

## The Project Team

### Wellesley Planning Board

Rose Mary Donahue, Chair (at project initiation)  
James C. Meade, Vice Chair  
Robert L. Dischino, Secretary  
Patricia Grandieri, Project Liaison  
Robert C. Sechrest  
Thomas Frisardi, Associate Member

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James C. Meade, Vice Chair  
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# Executive Summary

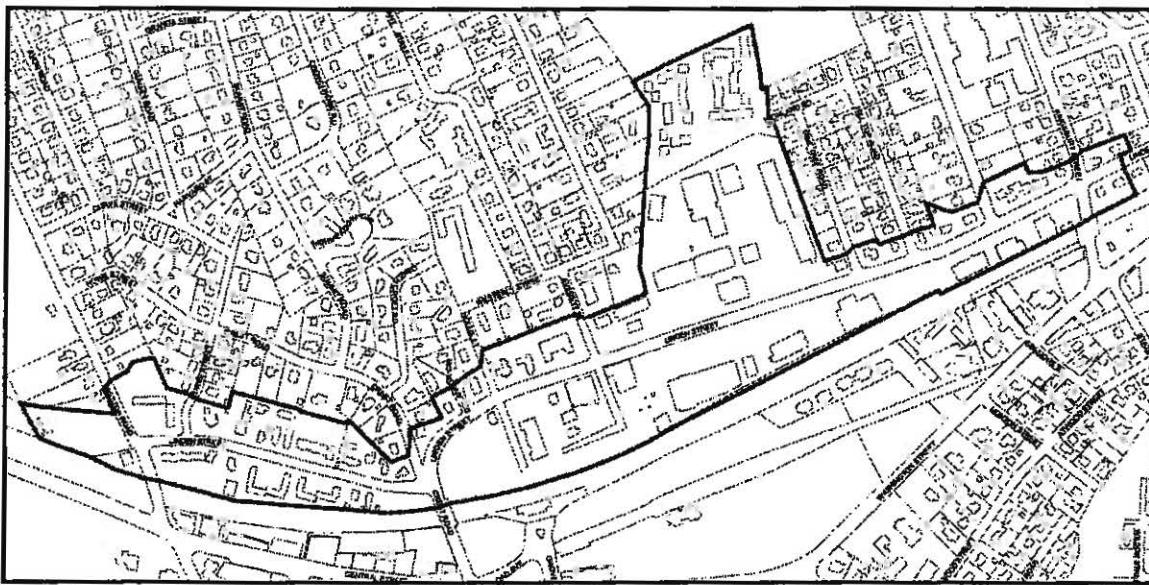
## Project Objective

The primary objective of this project is to create a Linden Street Corridor Master Plan to implement the ideas developed in a 2000 visioning process. The primary objective is to protect and improve public safety, operations and visual character, and economic vitality of the project area and residential neighborhoods. To develop the Master Plan, significant input was sought and partnership opportunities were identified between the public and private sectors to identify strategies for improving the area with regard to traffic and parking, the pedestrian environment, and streetscape aesthetics. Also, to promote an appropriate mix of business and residential uses, identify redevelopment opportunities, and suggest alternative land use regulations.

### Scope of the Linden Street Corridor Master Plan

- Identify various options for the layout of travel and bicycle lanes, sidewalks, and roadside landscaping to affect changes in traffic volume and speed, and provide an attractive and safe pedestrian environment.
- Present layout and circulation options for cars and pedestrians in the commercial area business parking lots.
- Explore the potential for infill development to lend greater visual coherence to the area.
- Explore options for a pedestrian connection over (or under) the MBTA tracks between Linden Street and Town Hall.
- Provide alternatives for decking the Tailby Parking Lot and further design options for the proposed Weston Road Substation parking lot.
- Prepare an overall plan for the revitalization of the Linden Street Commercial Area based on creative ideas and rigorous public review.
- Evaluate current development patterns, zoning and other land use regulations. Identify potential new and redevelopment parcels with projected types and amounts of growth that may occur in the Linden Street Project Area
- Develop cost estimates for proposed public improvements and identify potential funding sources, and implementation schedules and strategies.

The Linden Street Project Area (illustrated in Figure 1) is generally divided into two areas: the residential neighborhood between Weston Road and Crest Road; and the commercial corridor between Crest Road and Kingsbury Street. The Corridor Master Plans and recommendations recognize the individual characteristics of these two areas.

**FIGURE 1: LINDEN STREET PROJECT AREA**

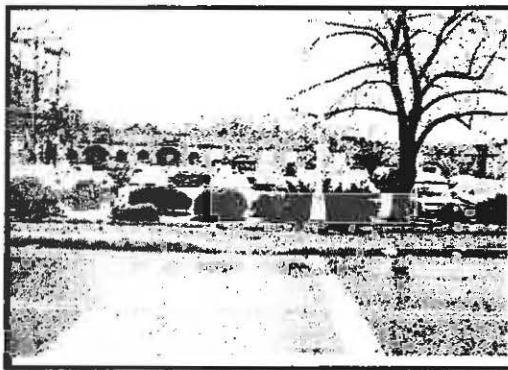
### Summary of Corridor Issues, Plans & Recommendations

#### Traffic Circulation & Parking

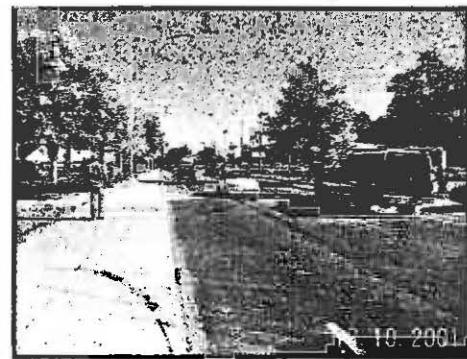
- Linden Street is used as a bypass to Route 16 and Central Street because of traffic congestion. This has resulted in conflicts between business, residential and through traffic.
- Traffic speed is a concern of Linden Street businesses and area residents. More signage and better enforcement of the speed limit is sought.
- Accident data reveals that there have been 40 accidents over a 2 ½ year period. A number of the recorded accidents have been rear end collisions, possibly indicating inattentiveness by drivers having difficulty in turning onto the street, and conflicts between residential and business traffic.
- Several properties have multiple curb cuts creating conflict points on the street and internal circulation problems. Some of these could be combined resulting in additional on-site parking spaces. Internal connections and common driveways should be encouraged where feasible.
- A maximum width requirement for driveways should be adopted to improve pedestrian safety. A twenty-eight (28) feet curb cut would be sufficient to accommodate most deliveries along the corridor. Curb cut changes need to be studied on a case-by-case basis as part of roadway redesign.
- Property owners want to maintain and add parking in the corridor where opportunities are provided. Potential additional parking is identified including the possible decking of the Tailby Lot, a new lot on the Weston Road Substation property, a parking structure located behind 141 and 151 Linden Street, and reconfiguring existing space on private parking lots.

## The Pedestrian Environment

- The pedestrian environment is somewhat unappealing due to high volumes and speed of automobile traffic, poor internal connections between businesses, deteriorating sidewalks, and few streetscape amenities.
- Existing crosswalks are not highly visible and do not adequately signal drivers to look for pedestrians.
- A pedestrian overpass crossing the railroad right-of-way would be very expensive given track clearance and handicapped accessibility requirements. A more feasible option would be an underpass, shown on the Corridor Master Plan in two potential locations.
- An improved pedestrian environment is sought as well as a more traditional atmosphere with a greater variety of uses (restaurants and small retail shops were mentioned specifically).



Crosswalk at 178 Linden St.



Sidewalk in front of 180 Linden St.

## Streetscape Improvements

- The Corridor Master Plan provides for 2 travel lanes of 11'6"; 2 gutter areas of 4 feet; and 7-foot sidewalk on each side of Linden Street in the commercial core between Kingsbury Street and Everett Street. The total cross section from curb to curb will measure 31 feet. Landscaped islands will also be expanded in this area.
- The Corridor Master Plan for the area between Everett Street and Weston Road recommends two travel lanes measuring 11'6" and 4-foot sidewalks on each side within the right-of-way.
- The Corridor Master Plan includes enhanced and expanded pedestrian-scaled features such as streetlights, sidewalk pavers, mature trees, quality signage, and landscaping.
- There are five utility poles located in the sidewalk around Everett Street, which are unattractive and pose accessibility problems. The Town should coordinate an effort between the Wellesley Light & Power Company and the local telephone company to replace these power lines with underground utilities.
- Private landscaping and plantings should respect the integrity of the street, should not obscure buildings, and should allow views to and from streets and sidewalks.



Planting strip in front Roche Brothers Supermarket

### Land Use & Regulatory Analysis

- The Linden Street Corridor Study Area includes about 46.6 acres of land. There are approximately 81 residences and 167 businesses in the area. The Linden Street Corridor is divided into four zoning districts as follows:
  - General Residential
  - Limited Residential
  - Business
  - Industrial
- Current zoning is not entirely reflective of the existing land uses, which are approximately one-half commercial and one-half residential.
  - A significant portion of the Business Zone is occupied by multi-family housing (Linden Square Townhouses behind Diehl's Home Center);
  - Limited Residential District is primarily used for public parking (the Tailby Lot); and
  - The Industrial District is utilized primarily for commercial uses.
- Wellesley has *cumulative zoning*, meaning that what is allowed in lower intensity districts (i.e. General Residential) is also permitted in higher intensity districts (i.e. Industrial). While this zoning technique encourages mixed use in higher intensity districts, it does not necessarily provide for the optimal density of uses (i.e. a single family home in the commercial core of Linden Street would not be appropriate).



- Use and dimensional requirements for the General Residential (GR) District appear to work well and result in few conflicting uses or patterns of development.
- The Limited Residential (LR) District includes just two parcels totaling 2.16 acres. The Tailby Parking Lot covers almost 95% of the land area. Because public streets bind the LR District the setback requirement is 30 feet. This makes any type of residential development difficult.
- Permitted uses and dimensional requirements in the Business (B) and Industrial Districts are very flexible.
- Required front-yard setback (30 feet) and floor area ratio (.3) facilitate strip development.
- Major amendments to the existing zoning regulations in the Linden Street Corridor would be difficult and contentious. Any changes to the approval process for the study area would apply to other areas of Wellesley, and would be difficult to gain a consensus.
- Most participants felt that existing zoning (or at least the existing types of development) in the corridor was serving the area adequately.
- The primary concern raised by property and business owners in the study area was the lengthy review process. The combined review process by the Design Review Board, Planning Board and ZBA can take up to 6 months and, in a number of cases, much longer.

### **General Recommendations for Land Use Regulations**

- Expedite the development review process for smaller projects and those meeting the land use goals of the Town of Wellesley Comprehensive Plan.
- Reduce setback requirements in the LR District from 30 feet to 10 feet to allow better opportunity of multi-family development such townhouses and apartments in combination with parking improvements.
- Reduce front-yard setback requirements in B and I districts from 30 feet to 10 feet to allow for more side and rear parking lots.
- Eliminate the Floor Area Ratio (FAR) in B and I districts to provide more infill and multi-story buildings.
- Site Plan Review - Consider a new threshold for smaller commercial projects that meet the objectives of the district and where potential impacts can be resolved administratively or through performance standards.

### **Recommendations for Parking Requirements**

- Reduce the on-site parking requirement from 3.2 to 2.5 spaces per 1,000 square feet of gross building space.
- Substitute a portion of required on-site parking with nearby public parking lots if Tailby Lot is expanded in the future.
- Shared parking agreements between adjacent property owners should be encouraged where uses are compatible and have differing peak demand times.
- Provide for off-site parking agreement for employees.

### **Recommendations for Landscaping Requirements**

- Require new development and redevelopment of properties with frontage on Linden Street provide landscaping plans consistent with the Corridor Master Plan.
- All new trees should be planted at a minimum size of 2.5" to 3.0" caliper.
- All trees should be pruned up to a minimum of 6 feet above ground level.
- All trees should be heat, drought and salt tolerant.
- Existing trees within the public right-of-way over 6" in caliper should not be removed except with permission from the DPW and Town Horticulturist.

### **Future Development Scenarios**

- Stakeholders desire pedestrian, parking and alternative transportation enhancements that reduce the reliance on the automobile to access the Linden Street Corridor.
- Current regulations may be discouraging existing property and business owners from making property improvements.
- Higher density mixed uses are very possible in the commercial section of the corridor given the areas proximity to established commercial districts, civic uses, educational services, and the MBTA commuter rail service and station.
- Adopt a Mixed-Use Overlay District ordinance as a tool to combat strip development and facilitate a pedestrian-scale design. The overlay district would not replace the existing zoning districts but provide a more flexible alternative for new development and redevelopment. The proposed overlay district is designed to accomplish the following goals:
  - Encourage social, education, employment, recreation, and shopping opportunities in close proximity to residences.
  - Design streets and parking area to balance the needs of both automobiles and pedestrians.
  - Provide for building sizes and character that define streets and public spaces.
  - Reduce the need to expand infrastructure and automobile use.
  - Provide flexibility for property owners as an incentive to create attractive and high quality development, and maximize the efficiency of land use.

## Consensus Recommendations:

The planning process included review of many ideas and recommendations and elicited significant debate on the character of the corridor, vehicular and pedestrian safety, and service needs of corridor businesses. There were several proposals that developed a consensus among participants in the planning workshops.

- Post speed limit signs
- Study further limited changes to curb cuts
- Reconstruct sidewalks using concrete with brick accents
- Place overhead utilities underground
- Replace street lighting with historic fixtures
- Provide one yellow blinking pedestrian light at primary crossing
- Strengthen design of crosswalks for increased safety
- Enhance street tree plantings and planter maintenance.

# Section 1

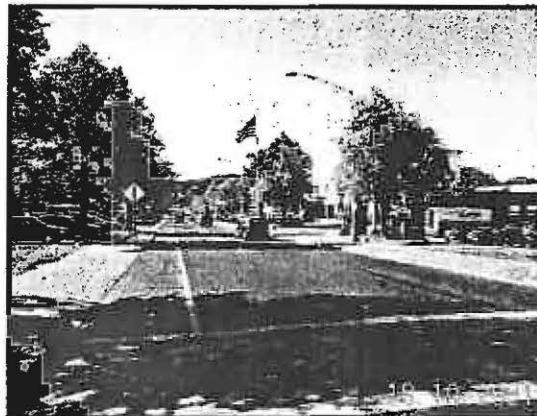
## Existing Plans, Policies & Regulations

### Existing Plans, Policies and Regulations

A detailed review of previous documents and studies, supplemented with field review, photography, and discussions with the working group and Town officials was conducted during the planning process. Wellesley previously completed a visioning process for Linden Street with extensive public involvement that served as the background for the direction taken in this study. In addition, interviews with Town officials and Linden Street stakeholders were conducted to bring new perspectives on existing plans and subsequent ideas.

The following plans, policies and regulations were reviewed for their impact on the Linden Street Corridor:

- Town of Wellesley Comprehensive Plan (1994)
- Linden Street Vision: Maintaining Character and Managing Change, Phase I (2001)
- Town of Wellesley Zoning Bylaws
- Town of Wellesley Design Guidelines
- Traffic Operations & Safety Study for Proposed Weston Road Parking Lot
- Sprague Elementary School Improvement Plans
- Draft Route 9 Corridor Study



Linden Street Corridor – Looking West

**Wellesley Comprehensive Plan** – The Comprehensive Plan (1994) outlined an initiative to evaluate issues and opportunities for Wellesley's commercial districts including Wellesley Square, Wellesley Lower Falls, Linden Street, Wellesley Hills, Cedar Street, Natick Line Western Gateway and State Street.

Specific land use and economic vitality goals and policies from the Comprehensive Plan that apply to Linden Street:

- Address the design details of transition areas between commercial and residential areas (Section II.5).
- Establish design themes for business districts and for the town as a whole, and implement them (Section II.9).
- Continue to identify Wellesley's commercial centers according to their functions, and reflect these functions in specific zoning regulations for appropriate uses and intensity within each commercial zoning district. Use the tools of zoning and design review to fine-tune the function, scale, character, and compatibility of design within each commercial area. (Section IV.1).
- Work jointly with the business community to document, analyze, and resolve parking problems (Section IV.2).
- Develop specific improvement programs for each commercial district in cooperation with property owners and business organizations. Provide more landscaping in commercial areas to beautify and define areas (Section IV.5)
- Coordinate vehicular traffic, pedestrian traffic and parking in commercial areas so that they function in an optimal manner. Orient retail business areas more to pedestrians than to automobiles (Section IV.6).

**Linden Street Vision Plan** – The Linden Street Vision Plan (Beals and Thomas, Inc.) was completed in December 2000. The study area encompassed the Linden Street corridor from Kingsbury Street to Weston Road. The Linden Street Vision Plan documented community concerns, ideas, and a series of recommendations and strategies for the future of the Corridor including the following:

- “Linden Street is a mix of residences and businesses that, for the most part, live harmoniously side by side”
- Maintain the character of the street – Residents, property owners and business owners generally like the zoning that has produced a mix of complimentary buildings that are patronized by nearby residences.
- Enhance pedestrian safety and calm the traffic – Area schools and senior housing produces a steady amount of pedestrian activity. There was a general consensus that automobiles drive too fast and without regard for pedestrian safety.
- Improve the pedestrian experience – Install ornamental lighting, landscaping, and possibly benches if they'll be used.
- Enhance the appearance of the street – Pedestrian and safety improvements will also result in a more attractive physical environment.
- Provide more long-term parking – Employee parking needs to be addressed. Participants recognized that employees used many of the spaces in the commercial areas. The Tailby Parking Lot, which has 288 spaces, could be expanded. Consider revising the parking ordinance to allow for overnight parking by area residents.
- Enhance the residential areas with streetscape improvements.
- Tie Linden Street to Wellesley Center – Connect these two districts through uniform design improvements.

## Land Use Regulation Analysis

Zoning ordinances are the primary implementation tools of local planning and should directly correspond to a community's master plan and other local planning documents. Unfortunately, conventional zoning is often an impediment to achieving these goals as land development compromises quality to adhere to rules that are inconsistent with planning policy. The results are fragmented and disappointing residential suburbs, highway oriented commercial strips, and deteriorating central business districts – all the direct product of zoning, subdivision, site plan and other conventional land use regulations.

**Zoning Districts and Current Land Uses** - The study area includes about 46.6 acres of land. There are about 81 residences and 167 businesses in the area. The Linden Street Corridor is divided into 4 zoning districts as follows:

- General Residential (GR) District
- Limited Residential (LR) District
- Business (B) District
- Industrial (I) District

The zoning districts are divided roughly into one-third industrial, one-third business, and one-third residential. However, current zoning is not reflective of the existing land uses, which are approximately one-half commercial and one-half residential.

<b>Table 1: Linden Street Corridor Zoning vs. Land Use</b>			
<b>Zoning District</b>	<b>Parcels</b>	<b>Total Acreage</b>	<b>Primary Use of Land</b>
General Residential	31	15.4 (33.0%)	Multi-Family Res.
Limited Residential	2	2.2 (4.7%)	Public Parking
Business	12	11.2 (24.0%)	Multi-Family Res.
Industrial	12	17.8 (38.2%)	Commercial Bus.
<b>Total</b>	<b>57</b>	<b>46.6</b>	

Town of Wellesley Assessors Records

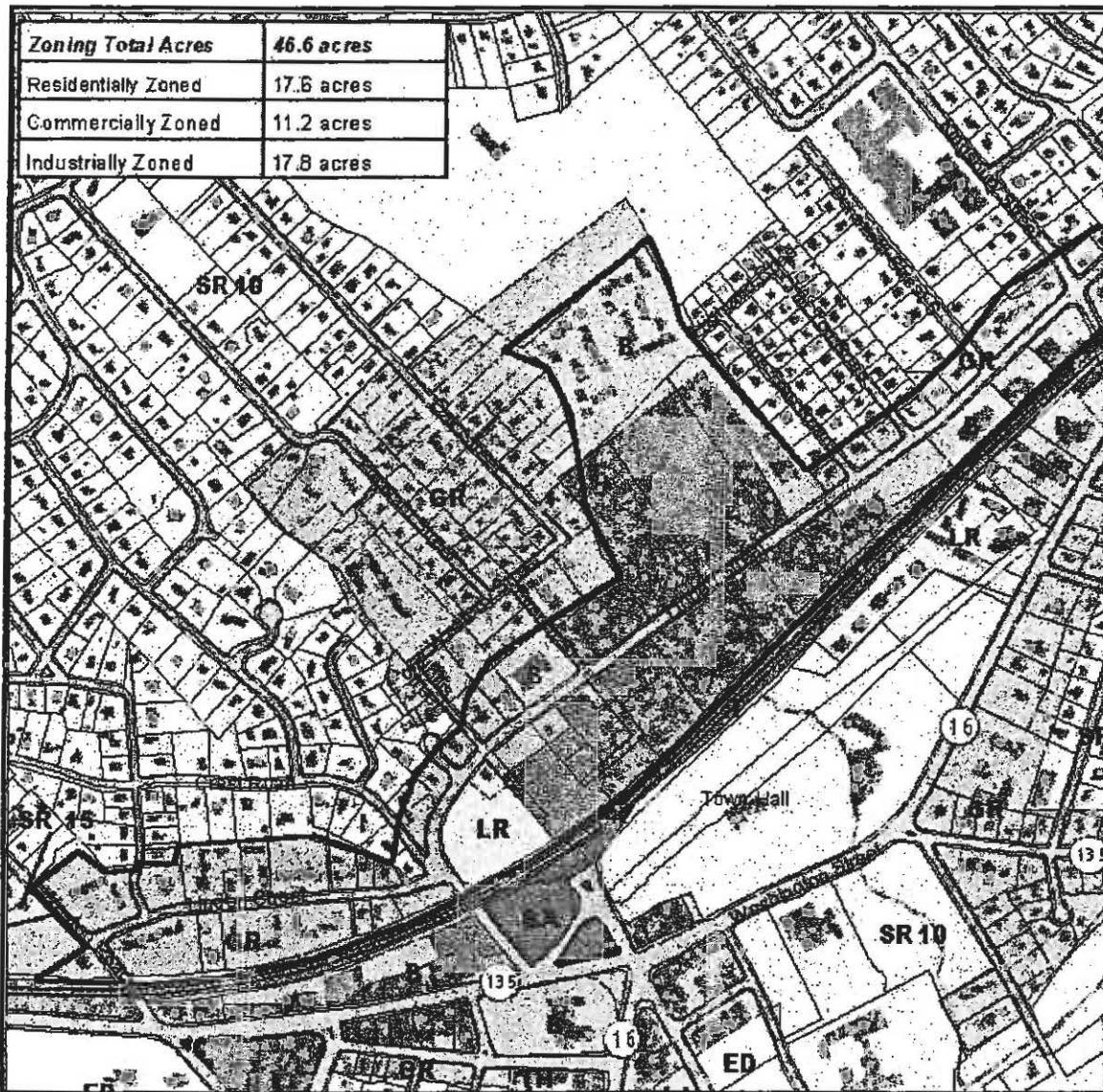
The current zoning reflects the previous uses and orientation of Linden Street toward the railroad, which historically played an important role in the commerce of Wellesley. Just 50 years ago, Linden Street was the primary area for industrial uses and services supported by the railroad. Today, however, there are few industrial uses that remain in the corridor and none that are serviced by rail directly.

<b>Table 2: Linden St. Corridor Existing Land Use</b>		
<b>Land Uses</b>	<b>Acres</b>	<b>% of Total</b>
Residential Uses	19.2	41.2%
Commercial Uses	24.4	52.4%
Municipal Uses	2.6	5.6%
Mixed Uses	0.4	0.9%
<b>Total</b>	<b>46.6</b>	

Town of Wellesley Assessors Records

The above table shows how the given zoning districts do not reflect the type of uses that exist in the corridor today. For example, a significant portion of the Business Zone is occupied by multi-family housing (Linden Square Townhouses behind Diehl's Home Center); the Limited Residential District is primarily used for public parking (the Tailby Lot); and all of the Industrial District is utilized by commercial uses.

**FIGURE 2: CURRENT ZONING AND LAND USE ON LINDEN STREET**



**Permitted Uses and Dimensional Requirements** - General regulations, as they apply to the zoning districts along Linden Street, are listed in Table 3 below. Wellesley has *cumulative zoning*, meaning that what is allowed in lower intensity districts (i.e. General Residential) is also permitted in higher intensity districts (i.e. Industrial). This zoning often results in conflicting land uses. For example, single-family homes that are built in

established commercial or industrial districts can be incompatible and reduce opportunities and interest in further private commercial development, which is usually the land use goal of the district.

The use and dimensional requirements for the **General Residential District (GR)**, which is located primarily on the western side of the corridor, appear to work well and result in few conflicting uses or patterns of development

The **Limited Residential District (LR)** includes two parcels totaling 2.16 acres. The Tailby Public Parking Lot covers almost 95% of the land area. This district is a key transition area between the Linden Street Corridor and Wellesley Square. It serves the parking needs of both districts as well as those commuting by train.

Residential development is appropriate and desirable in the LR district if; (a) it is combined with parking improvements, (b) it is architecturally compatible with surrounding area, and (c) it fills a particular housing need in the community, such as higher-density moderate to market rate apartments or townhouses. A well-designed residential development over a portion of the Tailby Parking Lot could screen parking lot expansion and add new customers to both commercial districts.

Because public right-of-way bound the LR district on all sides, the setback requirement is 30 feet. This seriously reduces the size of residential development or parking expansion that can occur on the parcel, reducing the economic feasibility of development. The setback requirement should be reduced to 10 feet to allow for parking improvements while also providing an opportunity for higher-density residential development.

The permitted uses and dimensional requirements in the **Business District (B) and Industrial District (I)** are very flexible. However, the required front-yard setback (30 feet) and floor area ratio (.3) are geared toward commercial strip development.

A 30-foot front setback would provide enough space for a single row of parking for a commercial building. Because most existing building and potential available land in the district are small parcels, property owners are inclined to provide all parking spaces in front of the building. A setback reduction to 10 feet would create more opportunity and incentive to utilize side and rear lot areas to accommodate parking needs. This is a key design characteristic of successful pedestrian-oriented mixed-use districts.

The existing **floor area ratio (FAR)** in the Industrial and Business zones is low for commercial districts and much too low for mixed-use districts. Under the current requirements, a 1-story building can only cover 30% of the lot area and a 2-story building just 15%. This low coverage requirement contributes more to the strip characteristics of the corridor than any other regulatory factor. To provide for new infill opportunities, reuse of the non-conforming building at 151 Linden Street, and facilitate 2 or 3-story mixed use development, the FAR should be eliminated from the standards. As long as the development meets the other requirements for setbacks, height, landscaping and parking there is no particular need for this requirement.

**Table 3: Linden Street Corridor Permitted Uses & Dimensional Requirements**

Category	General Residential	Limited Residential	Business	Industrial
<b>Uses</b>				
One-Family Dwelling	X	X	X	X
Religious Purpose Uses	X	SP	X	X
Public Educational Purpose Uses	X	SP	X	X
Child Care Facilities	X	SP	X	X
Agriculture, horticulture and floriculture (not including raising livestock for sale or loan)	X	SP	X	X
Club, except as customarily carried on as a business	X	SP	X	X
Home occupations (w/no outside storage, signage, employees, additional parking, etc)	X	SP	X	X
Other Educational Purpose Uses other than public	SP	SP	X	X
Public and semi-public institutions	SP	SP	X	X
Community group residences	SP	SP	X	X
Telephone exchange (no service yard or garage)	SP	SP	X	X
Earth extractions	SP	SP	X	X
Residence of 4 or more unrelated persons reside	SP	SP	X	X
Home occupations (w/2 FT non-resident employees and on-site parking)	SP	SP	X	X
Municipal parking facilities	SP	SP	X	X
Air Navigation facility (w/ two-thirds vote of town meeting)	SP	SP	X	X
Cemetery (w/majority town meeting vote)	BOH	SP	X	X
Municipal Light Plant or Water Works Plant (application by BPW)	SP	SP	X	X
Conversion of public use to permitted use in district (two-third vote at town meeting)	SP	SP	X	X
Two-family dwelling	X		X	X
Townhouse of 3 or more units (subject to specific dimensional requirements)	X			
Board house or lodge without restaurant	X			
Non-Profit housing of 3 units/building with 50% of residents 62 years or older	X			
Public housing for the elderly	X			
All uses allowed in SR, GR, or ED Districts				X
All uses allowed in SR, GR, ED or B Districts				X
Light manufacturing and assembly				X
Dry cleaning (w/ chemical solvents used)			SP	X
Motor vehicle sales and service			SP	X
Motor vehicle body repair and/or painting			SP	X
Sales of trailers, boats or RVs			SP	X
Sale/Rental of tools/equip. outdoors			SP	X
Retail sales with outside sales area			SP	X
Sales from trailers and vehicles			SP	X
Customary accessory uses with those permitted	X		X	X
Automated or mechanical car wash			SP	SP
Drive Thru windows			SP	SP

Similar Uses to those specified in the district			SP	SP
3-Family+, Apartment house, hotel, apt. hotel, hotel or inn			X	X
Restaurant, tea room or other eating place			X	X
Retail store <=50,000 SF of floor area and inside sales			X	X
Retail store >50,000 SF of floor area and inside sales			SP	X
Theater, hall, club or other places of assembly			X	X
Office, bank or other monetary institutions			X	X
Public or semi-public building			X	X
Motor vehicle storage, filling stations, repair shops, printing, telephone exchange			X	X
Veterinary hospital with 400 s.f. or less of run space			X	X
Light manufacturing where majority of product sold at retail on premises			X	X
<b>Dimensional Requirements*</b>				
Maximum Residential Density (SF/Unit)	7,000	2,500	N/A	N/A
Minimum Lot Size	28,000	50,000	N/A	N/A
Minimum Open Space	0.65	N/A	N/A	N/A
Maximum Lot Coverage by Building	0.2	0.2	N/A	N/A
Maximum Building Height	40	30	N/A	N/A
Minimum Lot Frontage	120	140	N/A	N/A
Minimum Front Yard Depth (Setback)**	35	30	30	30
Minimum Side Yard Width	25	30	N/A	N/A
Minimum Rear Yard Depth	25	30	N/A	N/A
Minimum Building Separation	30	N/A	N/A	N/A
Floor Area Ratio (ratio of building square footage to lot s.f.)	N/A	N/A	0.3	0.3
Site Plan Approval Required	X	X	X	X
On-Site Parking Requirements	X	X	X	X
Design Review	X	X	X	X

\* For relevant examples on Linden Street, in the GR District, a 4-unit townhouse scenario was used; in the LR District, a 20-unit non-profit housing development for mixed age groups was used.

\*\* On the Tailby Lot in the LR District, the lot is completely bound by public r-o-w so that the setback requirement is 30 feet for front, side and rear lot areas

**Site Plan Approval (Section XVIA)** - All construction other than 1 and 2-family homes requires site plan approval from the special permit granting authority (SPGA). The primary purpose of site plan review is to minimize additional traffic congestion on public and private ways. The following additional types of projects require site plan review:

- **Major Construction** – Changes to the outside appearance of a building and construction of 2,500 square foot (s.f.) or more; grading/grading/regarding and/or removal or disturbance of vegetation in an area of 5,000 s.f. or more; and construction within floodplains and water supply districts.

- **Minor Construction** – Exterior changes to a building and/or enlargement or alteration of parking or storage areas.
- **Project of Significant Impact** – Development with 10,000 s.f. or more of new construction; or 15,000 s.f. of alteration within a building of 15,000 or more of ground coverage.
- **Roadway Impacted by Development Traffic** – Development with more than 30 trips in a single direction in one hour.

Under the current requirements, small changes to the outside appearance of a building require site plan review as well as design review. The Town should consider a threshold for smaller commercial projects that meet the objectives of the district and where potential impacts can be resolved administratively or through performance standards.

**Off-Street Parking (Section XXI)** - Site plans must be submitted for any new or altered building showing that there is sufficient on-site parking. In Business and Industrial districts, the parking requirement for business, commercial, educational, and industrial uses is one parking space for each 150 square feet of ground floor area but not less than 3.2 spaces per 1,000 square feet of floor area in the building. This is a high requirement. Typically, parking requirements in commercial districts are between one space per 200 to 400 square feet. In mixed-use districts the requirements are typically lower because parking is more likely to be shared by uses with different peak demands.



**Private parking lot behind 141-151 Linden Street**

Other districts in Wellesley (i.e. Lower Falls and Wellesley Square) allow for public parking to substitute for on-site parking requirements if they are within 600 feet of any portion of the parcel and individually or jointly have 50 or more spaces. If this occurs, parking is reduced to 2.5 spaces per 1000 gross floor area of commercial floor area. There are currently no provisions allowing for off-site satellite parking for employees or shared parking agreements between adjoining buildings and uses where peak demand times differ.

Table 4: Linden Street Corridor Parking Requirements		
Uses	District	Minimum Number of Parking Spaces
Hotel, Inn, lodging house, restaurant, or other eating place	GR, LR	One space per 2 guest rooms or per 100 s.f. if food served.
Apartment building or group of buildings containing 3 or more dwelling units	LR	1 space on the lot for each dwelling unit
Town House	GR	2 spaces on the lot for each dwelling unit
Any building used for any business, industrial, educational or commercial purpose	B, I	1 space for each 150 s.f. of building ground coverage but not less 3.2 spaces per 1,000 s.f. of building floor area

If 15 or more spaces are provided or required on site, the *parking lot design* standards apply. Standards for 61 to 90 degree parking are: 8'6" space width; 18' space depth; and 24' aisle width. Compact cars can be used to satisfy up to 30% of required spaces with a standard 7'6"x15' dimension. These are standard parking design requirements.

Two (2) *driveways* per street line are permitted for each lot under this section of the Zoning Ordinance. This can create excessive curb-cuts and reduce vehicle and pedestrian safety. Driveway minimum width requirements are 12 feet for one-way and 18-24 feet for two-way traffic. In order to improvement the pedestrian environment, there should also be a maximum driveway width of 28 feet.

Paved parking areas must be setback 5 feet from any building wall, private or public way, and residential district, or used for residential, conservation or park purposes. The parking areas must also be constructed of durable, dustless, all-weather surfaces and provide curbing, wheel stops, or other devices to prevent driving into setback areas. These are important design requirements in a commercial area such as Linden Street.

The Town should consider amendments to the parking requirements as follow:

- Reduce the on-site parking requirement from 3.2 to 2.5 spaces per square feet of gross building space.
- Provide for substitution of on-site parking with nearby public parking lots if Tailby Lot is expanded in the future.
- Provide for shared parking agreements between adjacent property owners where uses are compatible and have differing peak demand times.
- Provide for off-site parking agreement for employees.
- Establish maximum width requirements for driveways to improve pedestrian safety.
- Internal connections and common driveways should be encouraged where feasible.

**Parking Lot Landscaping Requirements** - The following requirements apply to the Linden Street Corridor:

- For parking areas of 20 spaces or more, one tree per 10 spaces is required on any perimeter that abuts the sideline of a private or public way, or residential district or use.

- Ten (10) percent of a parking area has to be landscaped open space and at least 50% must be interior open space.
- Trees must be 2.5" caliper at and of a species of rapid growth and hardiness. Existing trees must be preserved to the extent practicable. Landscaped screening is required for parking lots abutting residential districts or uses including: dense growth of 5 feet in height and width; properly maintained; and shielded light.

The Town should consider additional frontage landscaping requirements that are consistent with the accepted streetscape plan for Linden Street. This should include the installation of sidewalks, planting strips, and landscaping materials where the plan is not completed. The minimum requirement of a 2-inch diameter tree is fairly small for an urban environment. The Town should consider requiring 2.5" caliper trees to improve the chances of survival and to provide immediate aesthetic impact.

The following landscaping recommendations are made for the Linden Street Corridor:

- Require new development and redevelopment to provide frontage landscaping plans consistent with the accepted streetscape plan for Linden Street.
- All trees should have a have a minimum caliper of 2.5" measured at 4.5 feet above ground level
- All trees should be limbed up to a minimum of 6 feet above ground level
- All trees should be heat, drought and salt tolerate. Recommended species include: Linden, London Plane, Ginko, Yellowwood, Honeylocust, and Red Maple.
- Existing trees over 6" in caliper should not be removed except with permission from the Tree Warden.
- Plantings should respect the integrity of the street. They should not obscure buildings but provide views to and from streets and sidewalks.

**Design Review (Section XXII)** - The Design Review Board reviews applications for sign permits, site plan approval and other projects specified by the Zoning Bylaws (such as other applications requiring a special permit). The Board is strictly advisory and follows the Wellesley Design Guidelines Handbook. The purpose of the design guidelines is to prevent blight, enhance natural and aesthetic qualities, conserve value of land and buildings, and protect and preserve historic and cultural heritage. DRB review is incorporated into the recommendations of the Planning Board and the SPGA. The following design review guidelines apply to the Linden Street Corridor as well as other zoning districts:

- A detailed advisory review of uses and structures having substantial impact on the Town.
- Design criteria include:
  - Preservation and enhancement of landscaping – preserve in natural state as much as practicable.
  - Relationship of buildings to environment – relate harmoniously to terrain and use, scale and architecture of existing buildings in vicinity including height, street façade, rhythm of solids and voids, spacing, materials, textures, and color, roof slopes, and scale.

- Open space – designed to add to the visual amenities of the area.
- Signs and advertising devices – size, locations, design, color, texture, lighting and materials.
- Heritage – minimize disruption, removal of traditional and significant historic uses and buildings.

**Signs and Identification Devices (Section XXIIA)** - The following requirements apply to the Linden Street Corridor:

- Signs must relate to the context of the building façade, pattern of the street front, and other permanent structures and elements in the immediate vicinity in terms of size, brightness, style, height and color.
- Signs must be sized and located so as to avoid obscuring architectural features
- Lettering height on secondary signs must be smaller than on the primary sign.
- Roof signs, moving signs and internally illuminated signs are not permitted
- Banner signs may be placed across public or private ways with permission of Board of Selectmen
- External illumination must be white, steady, stationary, light-shielded, and directed at the sign.
- All pre-existing non-conforming signs can be maintained but not altered unless to bring them into conformance with the current regulations.
- Standing signs must be setback at least 15 feet from the nearest property boundary and located on the same lot as the principal building.
- Temporary signs can be placed for up to 30 days and must be removed promptly after the given event.
- Projecting wall signs (perpendicular to the building) may not project more than 4 feet off the building and not over a public sidewalk.
- Window signs and panel signs are considered wall signs under the regulations.
- Design Review Board reviews all sign permits and makes advisory recommendations to the Building Inspector prior to issuing permit.
- Special permits may be granted for non-conforming signs if: scale is in reasonable relationship to vicinity and nearby structures; it defines or enhances architectural elements; is in harmony with other nearby signs; style is reasonably compatible; and does not present a safety hazard.
- SPGA shall not approve signs which: have more than 2 colors and are internally illuminated; wall signs exceeding the height of the top of a wall or 75 square feet in area; standing signs that exceed 10 feet in height or 50 square feet in area; or signs closer than 5 feet to nearest property boundary.
- Signs cannot be painted or posted directly onto a building but must have an intermediate surface except for individual lettering.

**Maximum Number of Signs Allowed** – No more than two of the following four types of signs are allowed for each business: wall, window, standing, or awning signs. If there is more than one entrance or a business is located in more than one building, an additional wall sign of the lesser of 25 square feet or 10% of the facade is permitted. Directory wall signs are permitted based on one square foot for each business or six square feet total, whichever is less.

**Maximum Allowable Sign Dimensions** - The Business and Industrial zoning districts along Linden Street are listed as "Commercial Districts" in the sign regulations. The maximum area of signage allowed for a given business is the sum of the areas of wall, window and awning signs of a business not exceeding the lesser of 10% of the facade of the establishment or 50 square feet. Specific sign dimensional requirements for this area are as follows:

<b>Table 5: Sign Requirements for Commercial Area of Linden Street</b>				
Sign Type	Height	Area	Number	Letter Height
Standing	6 feet	25 SF	1/Lot	14 inches
Wall	20 feet	50 SF*	1/Bus.	14 inches
Window	N/A	10 SF**	1/Bus.	8 inches
Awning	N/A	25% of awning*	1/Bus.	8 inches
Temporary Window***	N/A	25% of window	N/A	8 inches
Temporary	N/A	12 SF	1/Lot	N/A

\* or 10% of business facade area, whichever is less

\*\* or 10% of window area excluding doors if less

\*\*\* Paper poster board only

**The Development Review and Permitting Process** - The primary concern raised by property and business owners in the study area was the lengthy permit review process. A significant amount of time can be required for relatively small projects. Many of the commercial uses allowed in the Business and Industrial Districts must obtain site plan approval from the Planning Board and special permits from the Zoning Board of Adjustments. Additionally, design review is required for nearly all new development or external modifications.

The combined review process by the Design Review Board, Planning Board and ZBA can easily take up to 6 months and, in a number of cases, much longer. This can be a disincentive to existing and potential property and business owners who desire to make improvements or open new businesses.

The lengthy review and approval process is not just a Linden Street concern but applies town-wide. Any changes to the approval process for the study area would also apply to other areas of Wellesley, and would likely be difficult to gain consensus on. An alternative is to establish an *overlay district* for areas of the corridor targeted for new development, and simplify the review process by establishing performance standards. If new development projects are desired in the district and meet the standards of the overlay district, the review process should be expedited.

## Section 2

# Traffic Circulation, Parking & Pedestrian Safety

Good traffic circulation and pedestrian safety is critical to the long-term viability of any commercial district. The optimal scenario for traffic circulation in the Linden Street Corridor is slow and steady flow with good access to and between private properties. As one drives or walks through the corridor they must be able to easily identify buildings, businesses, and parking areas. If traffic is too slow, drivers will begin to avoid the area; if it's too fast, drivers have difficulty identifying and slowing down for store entrances and pedestrians. This section evaluates traffic circulation and pedestrian safety issues and opportunities in the Linden Street Corridor including the following:

- Vehicular traffic and circulation
- Potential traffic calming improvements
- Pedestrian safety and circulation
- Distribution of public and private parking
- Public and private streetscape design and landscaping

Traffic and pedestrian improvements must be scaled to the specific character of the various areas of the corridor. They must be balanced to serve the multiple needs and purposes of Linden Street. Improvements presented in this section of the plan address major corridor traffic and circulation issues, provide a safe and attractive environment for pedestrians, and identify opportunities for additional public and private parking.

### Traffic Circulation Issues & Improvements

#### Speed and Volume of Traffic

The “livability” of streets declines as volume and speed of traffic increase. Controlling traffic speed is important to pedestrian-oriented commercial streets. Residents and shoppers are more likely to walk, bike, and play along streets where speed is low to moderate.

Linden Street runs parallel to and north of Washington Street (Route 16), the eastern end of the Wellesley Square Commercial District. Given the traffic congestion in Wellesley Square and at the Washington Street/ Central Street intersection, and because until recently there was only one traffic signal on Linden Street, the corridor serves as a local bypass.

Much of the traffic does not use Linden Street as a destination and is traveling too fast. This causes the afternoon backup at the intersection of Weston Road by cars turning left from Linden Street. It also causes safety concerns in the commercial area of the corridor. Because of heavy traffic volumes, pedestrians have difficulty crossing Linden Street during certain times of the day.

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The combination of through traffic with business traffic in the corridor poses congestion and safety concerns to residents and property owners. With the addition of the new Spague School improvements, two intersections in the corridor have recently been signalized: Crest Road and Everett Street. Specific effects are not yet known but these additional signals should improve pedestrian safety as well as reduce the number of vehicles using Linden Street as a cut through.

### **Accident Statistics**

The Wellesley Police Department recorded 40 accidents in the Linden Street Corridor public way between May 1999 and January 2002. Over this 2½ year period, the distribution of these accidents were: three (3) from Weston Road to Crest Road; six (6) from Crest Road to Everett Street; 17 from Everett Street to Pine Tree Road; and 14 accidents reported from Pine Tree Road to Kingsbury Street. Slightly over half of the accidents (24 of the 40) were rear end accidents. This may indicate driver inattention, excessive speed and left turn movement in front of on-coming vehicles. These types of accidents are common on streets with a combination of business and through traffic such as Linden Street. The total number of accidents over this period is not considered excessive. However, residents and property owners alike want to make the corridor as safe as possible.

### **Access Management**

In the commercial section of the corridor between Kingsbury Street and Everett Street, there are an excessive number of curb cuts in proportion to the number of buildings. Several properties have multiple curb cuts in close proximity to each other. This can present safety problems as vehicles turn on and off Linden Street simultaneously at these driveways.

A number of the curb cuts on the corridor are excessively wide. While a typical cross section for a two-way curb cut is 24 feet, several are much wider. A wider curb cut facilitates higher turning speeds, as well as commercial delivery. They also expose pedestrians to traffic over a longer distance. Many curb cuts are not aligned directly across Linden Street from each other, which can make crossing the street more difficult for pedestrians.

A series of recommendations for curb-cut reconfiguration and reductions were discussed at all the project public workshops. This was probably the topic that garnered the greatest amount of disagreement among those in attendance. The goals were to consolidate curb cuts where feasible to improve pedestrian safety, enhance internal parking lot circulation, increases on-site parking spaces, promote opportunities for infill development, and improve automobile safety.

Participants at the first workshop were not in favor of reducing the number of curb-cuts in the corridor. They were also opposed to the concept of lining up curb-cuts to create 4-way intersections (and possible future signalization) for fear that this would increase the number of broad side accidents. Therefore, the number of curb-cuts was maintained in the recommended streetscape concept plan but several modifications were made including the following:

- Future redesign of the adjacent open curb west of Roche Brothers Supermarket that also serves Diehl Oil and parking.
- Width reduction of the bank entrance curb cut at 180 Linden Street to improve pedestrian safety, expand landscaping, and add parking spaces.
- Relocation of the outbound curb cut on the east end of the bank at 185 Linden Street to improve drive thru, provide separation between adjacent curb-cuts, increase on-site parking and landscaping opportunities, improve future infill opportunities, and possibly facilitate shared access to the adjacent Wellesley Volkswagen or other potential future uses.
- Reduce width of several curb cuts.
- Reduce the number of secondary curb-cuts (ex. 148 Linden Street access from Everett Street and onto Linden Street).

### **Traffic Calming**

Traffic calming measures are physical changes to roadways designed to reduce automobile speed to make drivers more alert to surrounding traffic conditions, pedestrians and bicyclists. Traffic calming measures may include, but are not limited to additional signage, speed bumps, chicanes, raised/textured mid-block pedestrian crossings, on-street parking and streetscape treatments.

The primary objectives in applying traffic calming techniques include the following:

- Make physical street improvements that enhance living conditions,
- Shift priorities to advocate street sharing by cars, pedestrians and bicyclists,
- Reduce traffic speeds,
- Reduce vehicular traffic volumes,
- Improve safety for cars and pedestrians.

Several traffic-calming measures have application for Linden Street and were discussed in the planning process. The goal of these traffic-calming measures is to reduce speed through design (not just posted speed limits). Design speeds between 20 and 35 mph are recommended. Speed limits must be self-enforcing, particularly on local streets. Several applicable traffic-calming techniques are included in the Linden Street Corridor Improvement Plan (Figure Plan 6).

The width of local streets is probably the most important factor in effective traffic calming. The Linden Street Corridor Improvement Plan recommends the reduction of the travel lane width slightly and adds neck downs (or curb extensions) at strategic locations to further narrow the roadway where pedestrian crossings are placed. Other future traffic calming devices recommended for Linden Street are shallow building setbacks and planting additional street trees.

A primary objective of the traffic-calming design on Linden Street is to make it less attractive to use for cut through traffic. This can be accomplished by the reduction in lane widths (or the perception of widths) and new traffic signals, making it physically

difficult to drive through at excessive speeds. These techniques combined with consistent traffic enforcement should reduce speeds and cut through use on Linden Street.

### Pedestrian Circulation & Safety Improvements

Pedestrian safety was the overriding concern in the Linden Street Vision Plan. The main concerns raised were the poor condition of sidewalks, potential dangers to pedestrians created by high speeds and wide curb cuts, possible pathways over the railroad tracks connecting the corridor with Town Hall; and the need for more traffic control signs and enforcement.

The existing sidewalk network on Linden Street varies widely in size, location and materials. In general pedestrian areas throughout the Linden Street corridor tend to be confusing and unclear. There are crosswalks along the corridor but they do not necessarily follow the “desire line” of many pedestrians. This tends to cause a high amount of jaywalking. Additionally, the painted crosswalks do not present a highly visible design feature to motorists.

The pedestrian design features in the Linden Street Corridor Improvement Plan emphasize the following planning objectives:

- Wider sidewalks with attractive new materials
- Stronger crosswalks using different materials that distinguish them from the asphalt on the roadway
- Flashing yellow pedestrian light system
- Pedestrian links connecting the public sidewalk to private parking lots and buildings
- Shorter crosswalks using neck downs to narrow the roadway
- Enhanced landscaping to improve aesthetics and reduce vehicle travel speeds

### Kingsbury Street to Everett Street

The commercial section of the corridor (between Everett Street and Kingsbury Street) includes a 5-foot sidewalk on the north side and an 8-foot sidewalk on the south side. This area has the highest potential amount of pedestrian traffic because of the close proximity of businesses, schools, and adjoining neighborhoods. However, pedestrian activity corridor-wide was not observed to be extensive.

The existing roadway dimensions of Linden Street promote excessive traffic speed through the commercial core of the corridor. Between Kingsbury Street and Everett Street, Linden Street has the following cross section from south to north:

- 6'landscaped planter owned ½ by the Town and ½ the property owner,
- 5'wide sidewalk,
- 34' wide roadway (two 13'-6" travel lanes and two 3'6" gutter strips),
- 8' wide sidewalk with street lights,
- 11' wide landscaped planter owned and maintained by the private owner.

Figure 4: Existing Linden Street Cross Section  
(Everett Street to Kingsbury Street)

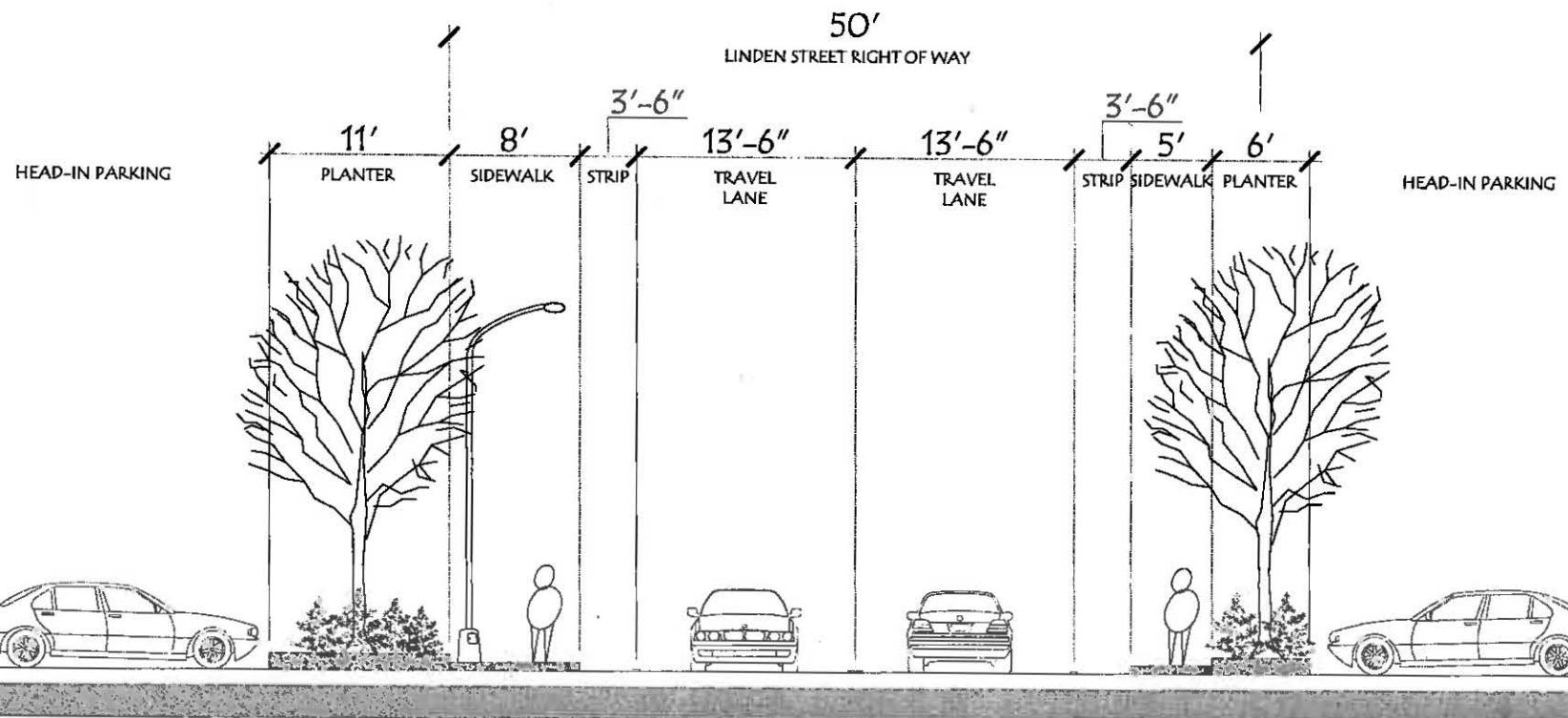
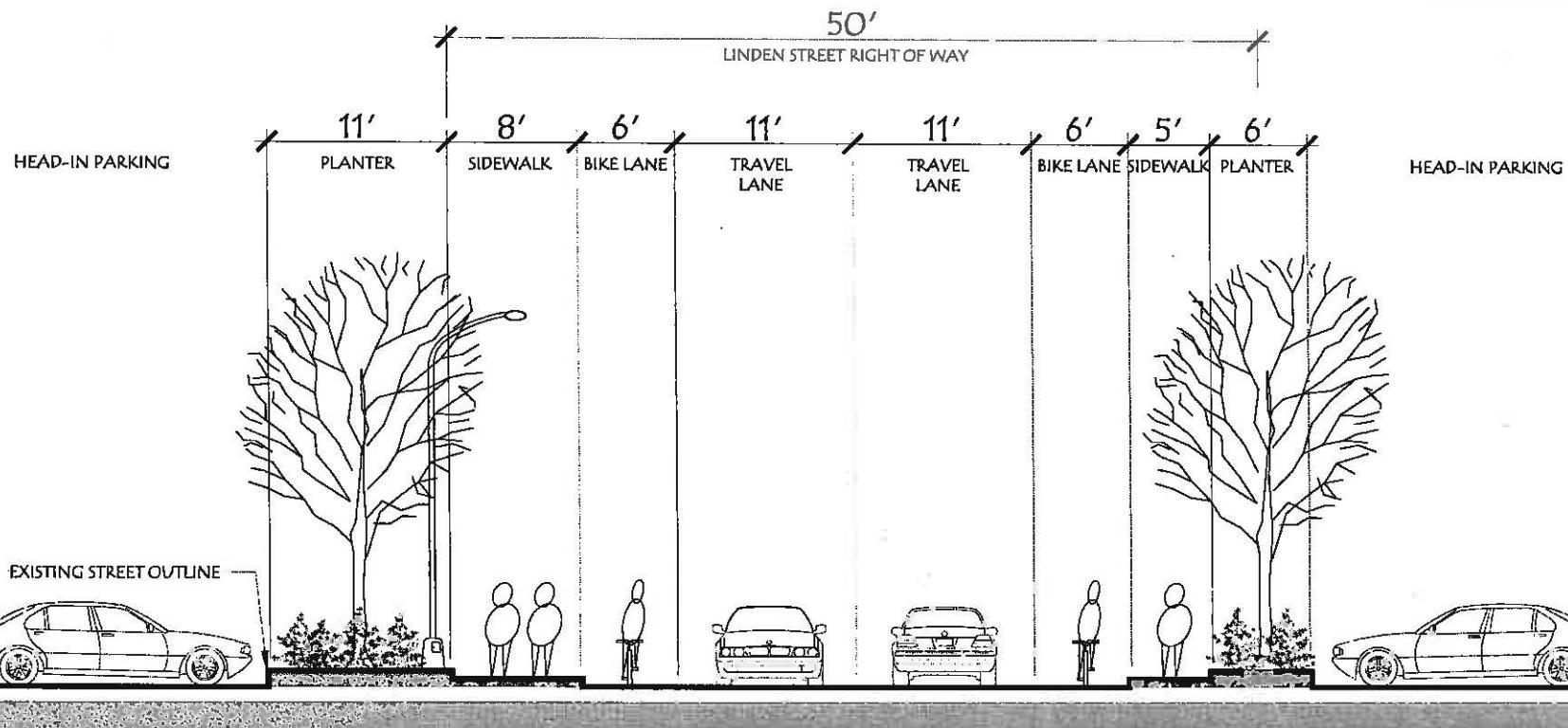


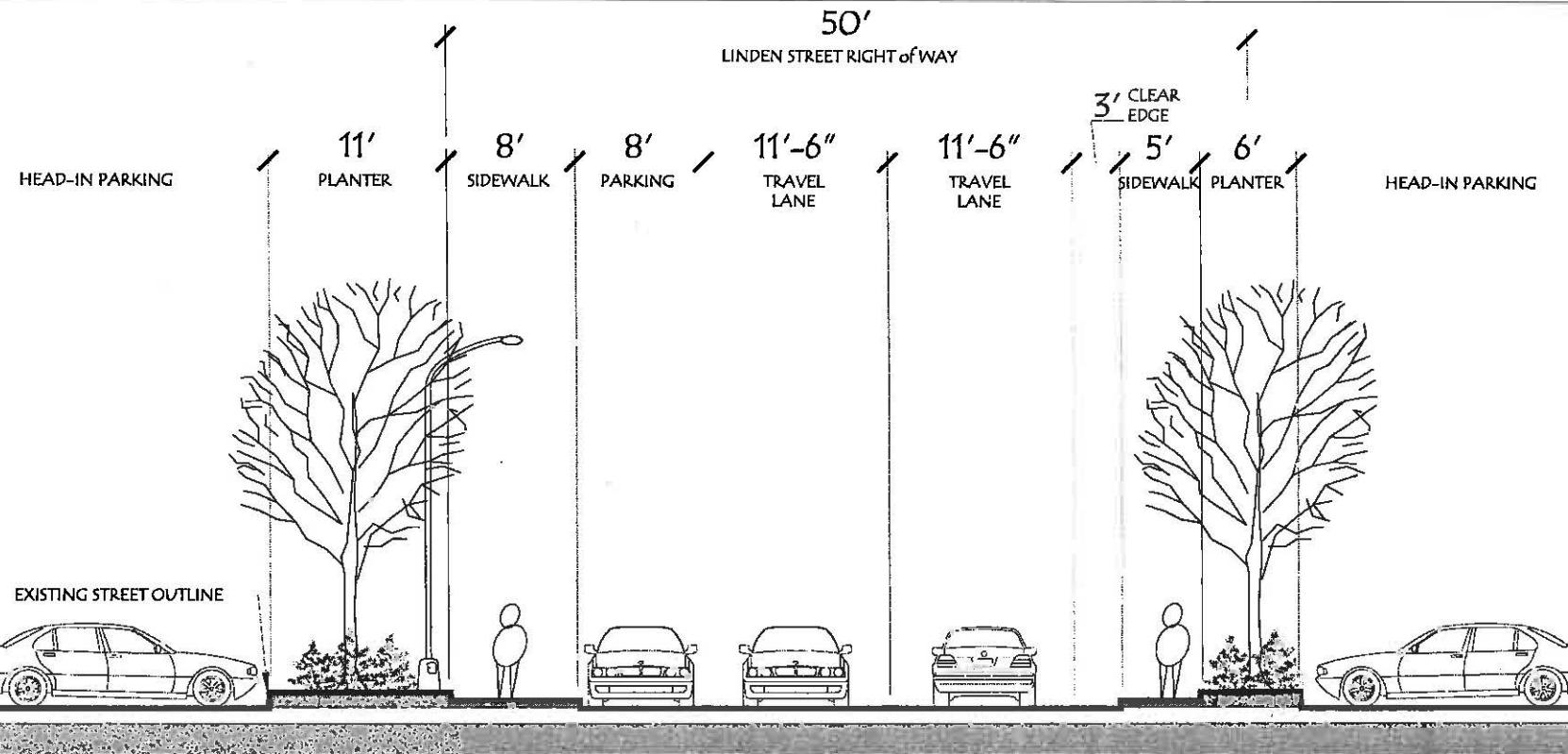
Figure 5A: Alternative Linden Street Cross Section 1  
Restripe Street for Bikeway / Narrow Street



- Narrow Travel Lanes from 13'-6" to 11'
- Preserve Planter Green Strip with Cut-Ins to Interior Sidewalk
- Create Bikeway
- Slow/Calm Traffic, Improve Pedestrian Safety (less width at crossing)
- Important Aesthetic Element
- Safety for Bicyclist, Adequate Separation from Vehicular Traffic and Pedestrians



Figure 5B: Alternative Linden Street Cross Section 2  
On-Street Parking One Side

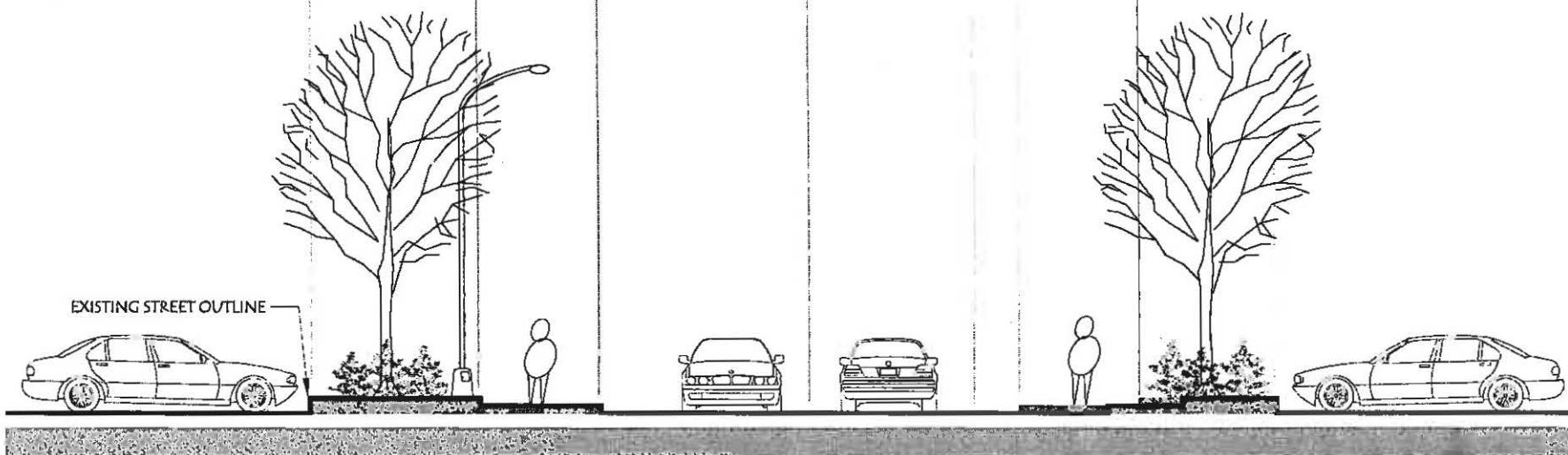


- Narrow Travel Lanes from 13'-6" to 11'-6"
- Preserve Planter Green Strip with Cut-Ins to Interior Sidewalk
- On-Street Parking on West Side of Street
- Slow/Calm Traffic, Improve Pedestrian Safety (less width at crossing)
- Important Aesthetic Element
- Supplement Parking to Provide Infill Development Opportunities, Buffer Between Pedestrian and Vehicular Traffic, Traffic Calming



Figure 5C: Alternative Linden Street Cross Section 3  
Narrow Street / Expand Sidewalk

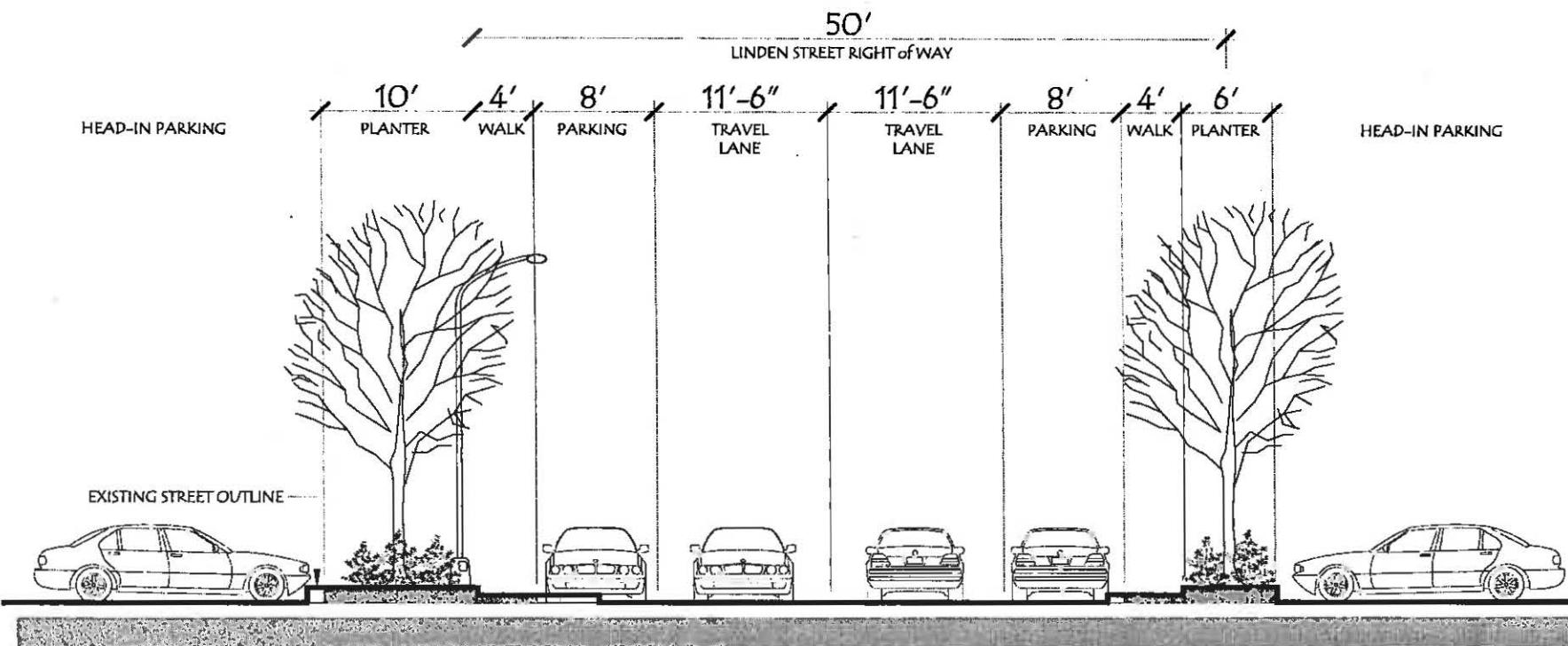
50'  
LINDEN STREET RIGHT of WAY  
4' GUTTER  
11' 7' 11'-6" 11'-6" 7' 9'  
HEAD-IN PARKING PLANter SIDEWALK TRAVEL LANE TRAVEL LANE SIDEWALK PLANter HEAD-IN PARKING



- Narrow Travel Lanes from 13'-6" to 11'-6"
- Widen Sidewalks
- Preserve Planter Green Strip with Cut-Ins to Interior Sidewalk
- Slow/Calm Traffic, Improve Pedestrian Safety (less width at crossing)
- Balance Pedestrian and Vehicle Use of Corridor, Accommodate Anticipated Increase in Pedestrian Activity
- Important Aesthetic Element



Figure 5D: Alternative Linden Street Cross Section 4  
Widen Street / On-Street Parking Both Sides / Move Sidewalks inside Planting Strips



### ALTERNATIVE IDEAS

- Narrow Travel Lanes from 13'-6" to 11'-6"
- Provide Interior Sidewalk
- Preserve Planter Green Strip with Cut-Ins to Interior Sidewalk
- On-Street Parking Both Sides

### OBJECTIVES

- Slow/Calm Traffic, Improve Pedestrian Safety (less width at crossing)
- Buffer Pedestrians from Vehicular Traffic, Improve Walking Connections to Commercial Buildings
- Important Aesthetic Element
- Supplement Parking to Provide Infill Development Opportunities, Buffer Between Pedestrian and Vehicular Traffic, Traffic Calming



Linden Street Corridor Plan

WELLESLEY, MA

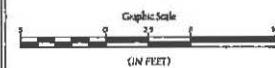
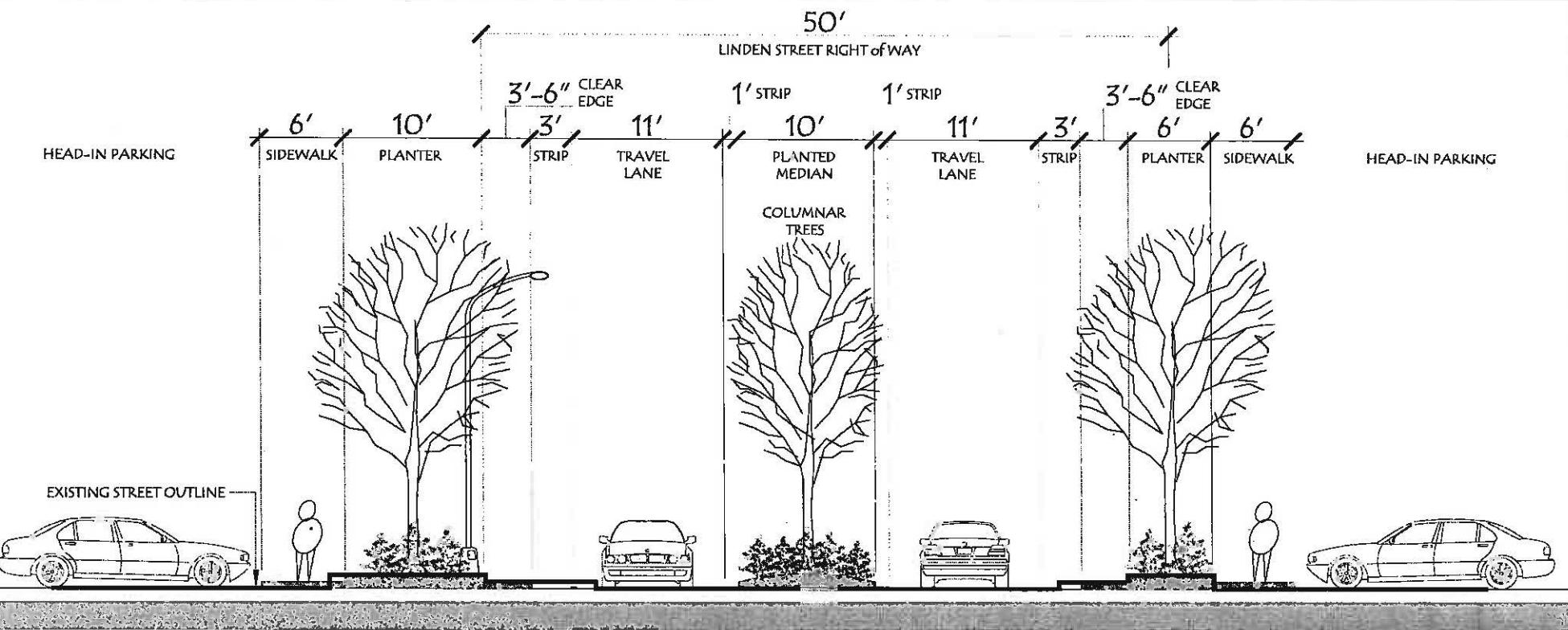


Figure 5E: Alternative Linden Street Cross Section 5  
Widen Street / Provide Planted Median / Move Sidewalks inside Planting Strips



### ALTERNATIVE IDEAS

- Narrow Travel Lanes from 13'-6" to 11'
- Provide Interior Sidewalk
- Preserve Planter Green Strip with Cut-Ins to Interior Sidewalk
- Provide Boulevard Treatment (intermittent)

### OBJECTIVES

- Slow/Calm Traffic, Improve Pedestrian Safety (less width at crossing)
- Buffer Pedestrians from Vehicular Traffic, Improve Walking Connections to Commercial Buildings
- Important Aesthetic Element
- Traffic Calming, Refuge for Pedestrians Crossing the Street, Attractive Landscape Element, Channel Turning Movements



Both sidewalks in the commercial section are separated from business parking lots by a landscaped planting strip ranging from 6 to 11 feet wide. There are no planting strips between the sidewalks and roadway, and pedestrians are separated from moving traffic only by curbing and a 3'6" striped apron. Sidewalks in this area are constructed of concrete bricks and are generally in poor condition.

Five (5) separate alternative improvement plans were developed for this section of Linden Street and presented at the first public workshop(Figures 5A-5E). Each incorporates traffic calming techniques aimed at slowing traffic and improving the pedestrian environment:

- Alternative 1 – Restripe existing pavement dimensions with inclusion of bicycle lanes (11' travel lanes and 6' on-street bicycle lanes)
- Alternative 2 - Addition of alternate on-street parking one side (11'-6" travel lanes with 8' wide on-street parking lane)
- Alternative 3 – Narrow roadway with expansion of sidewalks to 8' with 10'6" travel lanes,
- Alternative 4 – Widen roadway and add on-street parking on both sides (11'-6" travel lanes and 8' wide parking lanes)
- Alternative 5 – Addition of a planted median with sidewalk relocation (11' travel lanes, 10' planted median, 6' internal sidewalks)

Initially on-street parking was considered to slow traffic, and provide more parking and infill development opportunities along Linden Street. However, this concept was rejected during the first workshop. Figure 6 shows the recommended cross section through the commercial corridor. It features a 31-foot wide roadway cross-section, seven-foot wide sidewalks on both sides, and a wider planting strip on the south side. This will provide two 11'-6" wide lanes which reduces the visual perception of the vehicle travel way, encourages traffic to slow down, and reduces cut-through traffic.

### **Everett Street to Crest Road**

This segment of the Linden Street corridor is a transition area between the commercial areas to the east and the residential area to the west. There is significant traffic and turning movements in this segment due to the connection to Route 16 and Wellesley Square from Crest Street, access to the Tailby Parking Lot, access to the new Sprague Elementary School, and other connections to adjacent neighborhoods. Two signalization projects were completed in the summer of 2002 at the intersections with Crest Road and Everett Street. These improvements should significantly improve pedestrian and vehicle safety in the area.

### **Crest Road to Weston Road**

This segment of Linden Street is a medium density residential neighborhood. Because of the congestion in Wellesley Square, cut-through traffic is a major concern of residents in this area.

The cross section of the western section of the corridor includes two 12-foot travel lanes, asphalt curbs, grass strips varying from 2-4-feet, and 4-foot asphalt sidewalks located on each side. There are numerous driveways and one side street, which access Linden Street along this segment. The Linden Street Corridor Plan recommends that street trees be planted behind the sidewalk on private property in this area because of limited space in the right-of-way and to avoid problems of site lines. It is also recommended that asphalt sidewalks should be replaced with concrete and asphalt curbing be replaced with granite. **A right turning lane onto Weston Road and no left hand turns between the hours of 6:30- 9:00 AM. and 3:30-7:00 PM. is also recommended.**

### **Internal Pedestrian Improvements**

Internal pedestrian circulation in the private parking lots is a major concern in the commercial area of the corridor. There are few internal or separated sidewalk corridors from building to building, between parking areas and building, and between the public sidewalk and buildings. The goal is to provide safe and attractive pedestrian connections between buildings, parking lots and the public sidewalk without losing parking spaces.

Creating a safe pedestrian corridor from the public sidewalk to the storefronts was identified as an important design objective in the planning process. Figure 7 illustrates this component of the recommended improvement plan. This internal pedestrian corridor is perpendicular to the roadway and connects Roche Brothers Supermarket to 168 Linden Street. As the plan shows, this can be established without a reduction in the total number of on-site parking spaces.

### **Pedestrian Bridge Feasibility**

The primary purpose of a pedestrian crossing over the railroad tracks is to provide a safe access to Linden Street from the Town Hall, Hunnewell Park and the adjacent residential neighborhood. Through the Linden Street visioning process, public comments from the workshops and discussions with the Massachusetts Bay Transit Authority three potential opportunities were explored:

- 1.) An above grade crossing in the proximity of Wellesley Volkswagen and the housing complex, Morton Circle (505 Washington Street), on the south side of the railroad tracks.
- 2.) An above grade crossing near the Wellesley Square Post Office
- 3.) An underpass at the old Everett Street right of way connecting the area behind 151 Linden Street to Town Hall directly

Discussions with the MBTA were held to determine design criteria for a bridge including overhead clearance, right-of-way setbacks, and missile barrier requirements. Based on these discussions the following criteria were established:

- At-grade access will not be considered
- Overpass clearance must be 22 feet above the railroad tracks for double stacked trains

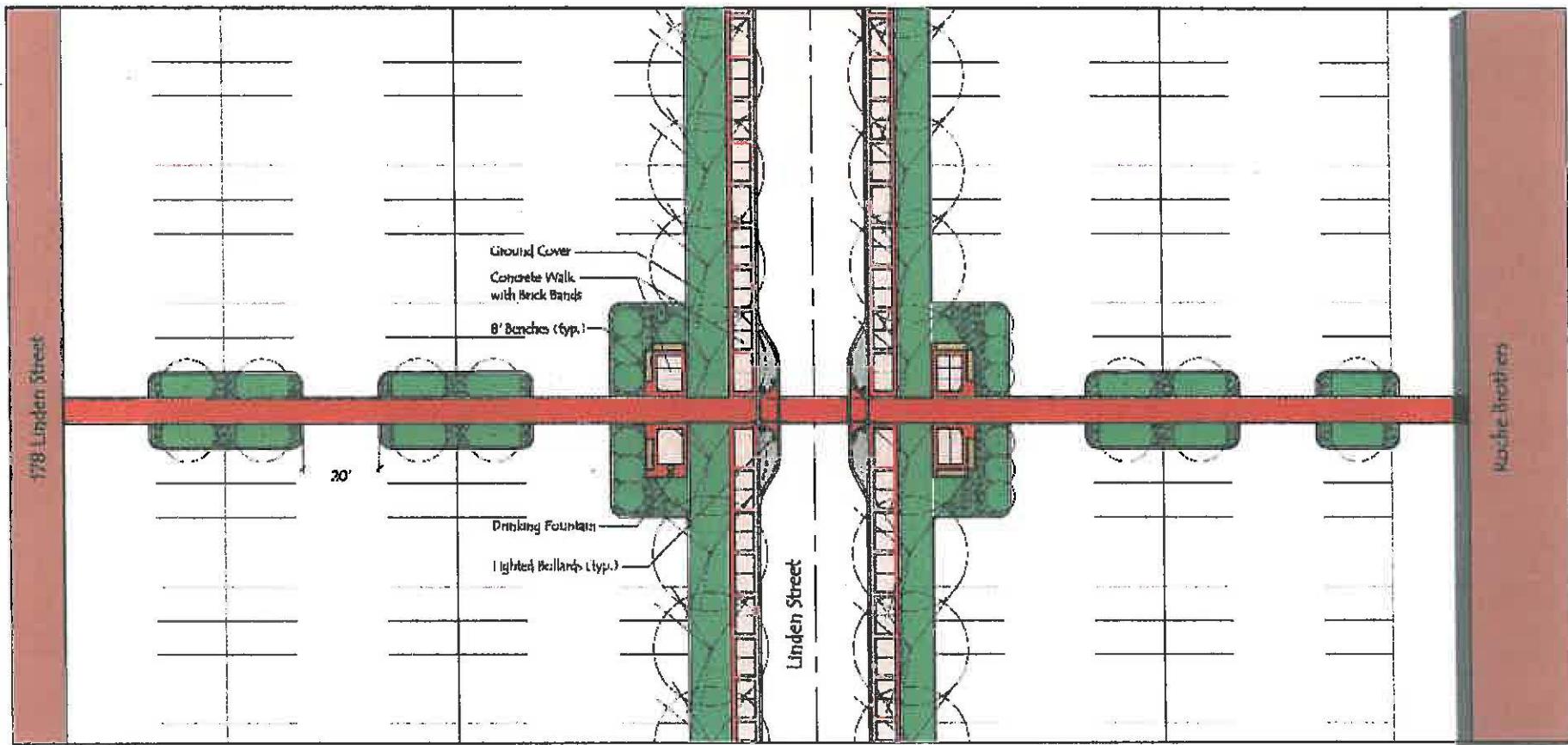
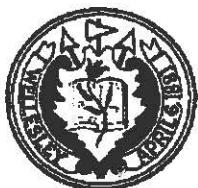


Figure 7A: Conceptual Internal  
Pedestrian Connection Plan

Wellesley, Massachusetts



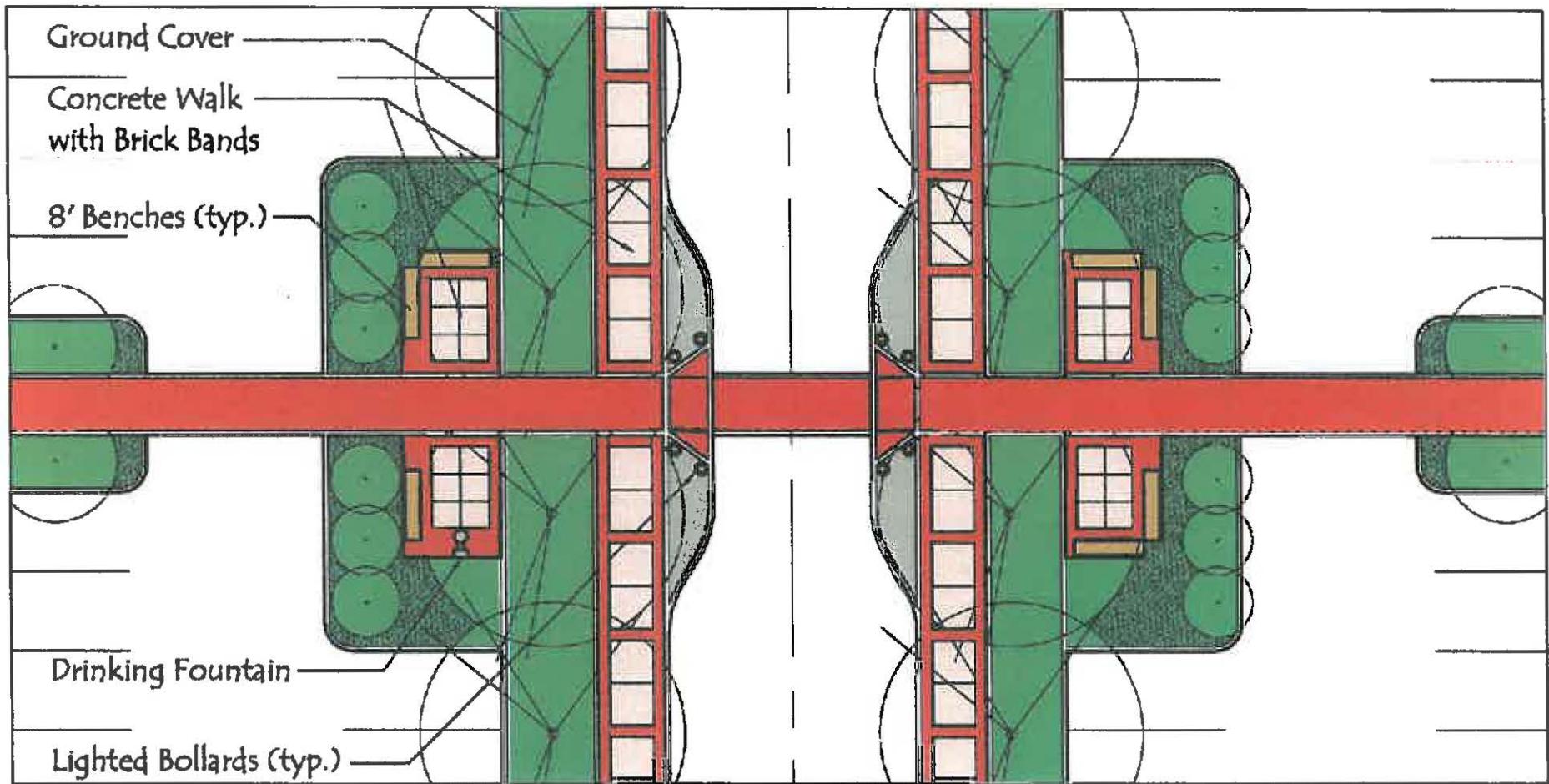


Figure 7B: Conceptual Internal  
Pedestrian Connection Seating Area

Wellesley, Massachusetts



- Bridge landing must not be located in the railroad right-of-way
- An overpass must be handicapped accessible and meet all applicable codes
- An underpass is the preferred approach

A pedestrian bridge would be quite expensive and unattractive due to extensive ramping. The handicapped requirements would require in excess of 300 feet of ramping in order to meet the minimum track clearance. This would require a significant amount of land for construction.

The preferred option is to construct an underpass, which would require less clearance and make it easier to meet handicapped accessibility requirements. An underpass would also be considerably less expensive and require less land area for development. The optimal location for a pedestrian underpass would be at the end of Everett Street where the grade rises to the tracks. This location would provide direct access to Town Hall and could potentially be incorporated into a new parking structure over the existing parking lot at this site.



*An example of a pedestrian underpass; Bridgewater State College*

### Parking Issues & Improvements

Parking dominates the landscape in the commercial section of Linden Street. There are approximately 1,069 total parking spaces in this area including 208 public spaces (at the Tailby Lot) and 861 private spaces.

General on-site observations indicate that private parking is generally well distributed throughout the corridor and that parking spaces were readily available in most lots throughout the day. The few areas where there appeared to be a parking strain were at Roche Brothers Supermarket and on the east end of the corridor near Kingsbury Street during peaking PM hours. Alternatively, some parking areas within the district appeared to be less than half full for substantial periods of the day such as the lot abutting the railroad tracks behind 151 Linden Street.

The significant amount of private off-street parking in the commercial area is due primarily to strict parking requirements in the Zoning Ordinance rather than actual measurements of need and use. However, most property and business owners appear content with the amount of parking, and would even like to expand parking where opportunities exist.

Several issues were raised and discussed at the second public workshop regarding parking needs and potential improvements:

- Determine the size of parking lots and structures based on reasonable demand
- Provide for peak parking and overflow areas
- Encourage and plan for shared parking among adjacent uses
- Create well-designed and landscaped parking lots
- Improve internal circulation and parking patterns within lots
- Place parking on the side or behind buildings to reduce the visual blight of endless parking lots.

The availability of parking, and the perception of adequate parking, is critical to the success of any commercial area. The recommended improvement plan includes several public and private parking strategies for Linden Street.

### **Public Parking Improvements**

**Tailby Parking Lot-** This municipal lot was evaluated for opportunities to expand the number of spaces and use of the site to meet other community objectives. Two alternative conceptual designs were developed for the Tailby Lot which are shown in Figures 9A and B below.

The first alternative is to construct a new deck over the existing parking lot. An asset to decking is the existing grade difference between Crest Road and the surface of the Tailby lot. This provides an excellent opportunity for the deck to fit into the existing landscape with minimal aesthetic impact. Additionally, the new signal recently installed on Crest Road could include a phase in the future for a new entrance to a decked structure. This would provide complete external access to both levels without internal ramping, maximizing the use of space. This alternative could provide a total of 406 parking spaces.

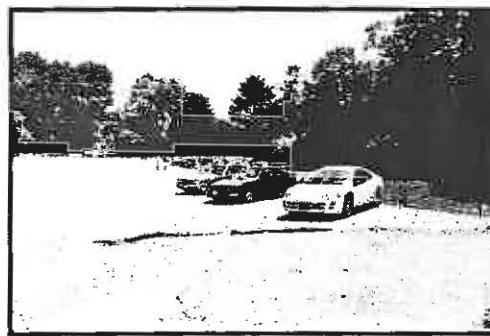
The second alternative includes a new parking deck and 18 housing units at the street level off Crest Road. This would reduce the number of total available parking spaces to 343, which would still provide a gain of 135 new spaces. Additionally, the new housing units could be constructed by the Town and provide affordable housing opportunities to local residents. The sale or rent of these units could also be used to offset the cost of constructing the new parking deck.

During the planning process a number of concerns were raised regarding the current use of the Tailby Lot that should be addressed in the short-term. A common issue raised was the number of commuters from out of town using spaces in the lot. A recent study

conducted by the Town estimated that non-residents use over half of the spaces on a daily basis. Some possible short-term approaches are as follows:

- Revise the parking rate structure by increasing the fee for commuter parking. The current parking rate is only \$2 per day and could be raised to \$4 or higher, while still being attractive for commuter use.
- Change the timing of a number of spaces in the parking lot to two hours to accommodate local parking needs generated by Town Hall, Wellesley Square and Linden Street.
- Rope off a percentage of the spaces until after commuting hours.
- Establish a resident sticker program for the Tailby Lot and allocate a sufficient number of parking spaces at a reasonable cost to local commuters.

**Weston Road Substation Parking** - A conceptual schematic design for a new parking lot on the Weston Road Substation site is illustrated in Figure 8 below. The main objective of this facility is to relieve parking congestion in the Wellesley Square area.



**Weston Road Substation Site**

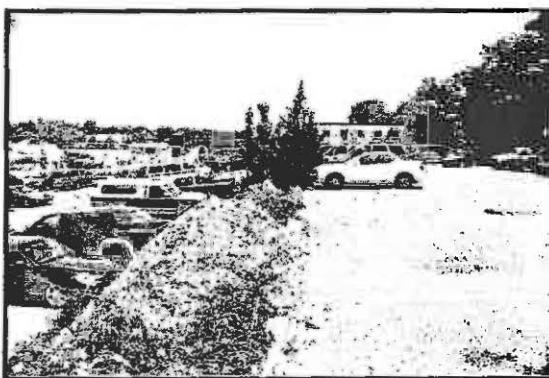
It is envisioned that employees of the Central Street businesses would be the primary users this new parking lot. Spaces could be leased to area merchants and used by their employees. Because of its remote location, most shoppers would tend not to use this site. However, as more Wellesley Square employees use this new lot more spaces in the Square would remain available for customer parking. Additional uses of this lot may be residential tenants in the Linden Street area for overnight parking.

One of the main concerns with the development of this site is the Weston Road/Linden Street intersection, which is presently at Level Of Service (LOF) F and would need to be signalized if access into the parking were to be relocated across from Linden Street. Additionally, poor sight lines exist along Weston Road looking to the north. This will require further study to determine the safest access options to serve the lot. Additionally, it will be important to determine the potential business and resident market for this lot prior to construction.

### Private Parking Improvements

A number of private parking lots in the commercial section of Linden Street could be reconfigured to improve internal circulation and add new spaces. The recommended improvement plan illustrates such potential changes in the lots located at 165, 178, 180, 185, and 200 Linden Street. Reconfiguration of these lots was made through a series of changes to the general layout of parking spaces, aisles, curb cuts, internal circulation, and landscaping improvements.

The private parking lots located off of Everett Street Extension behind 141 and 151 could also be greatly increased by the construction of a new parking structure. These lots are located in an area of the corridor where private parking would be well utilized today and well as in the future as new development and redevelopment occur on Linden Street. Through agreements with the Town and adjacent property owners, this new structure could also serve Town Hall and MBTA commuters. The grade of the site lends itself well to a new parking structure that potentially could accommodate 300 vehicles. This proposed structure is identified on Figure 10: 10-Year Potential Development and Redevelopment Scenarios in the Linden Street Corridor.



Potential parking structure location behind 151 Linden St.

### Streetscape Design and Landscape Improvements

The Linden Street Corridor Improvement Plan envisions several improvements to the character and appearance of the Linden Street right-of-way as well and to the private parking lots along the corridor. By improving the visual character of the corridor, it will be a more attractive and comfortable place to work and shop, enhancing the value of the area.

Sidewalks should be constructed of cement concrete with decorative banding of brick unit pavers. This treatment should be similar in character to Wellesley Square, and will replace the old concrete pavers that have weathered and settled unevenly since first installed. The unit paver treatment should be repeated at the crosswalks, at paved plazas within the pedestrian node, and in the pedestrian corridor through the parking lots.

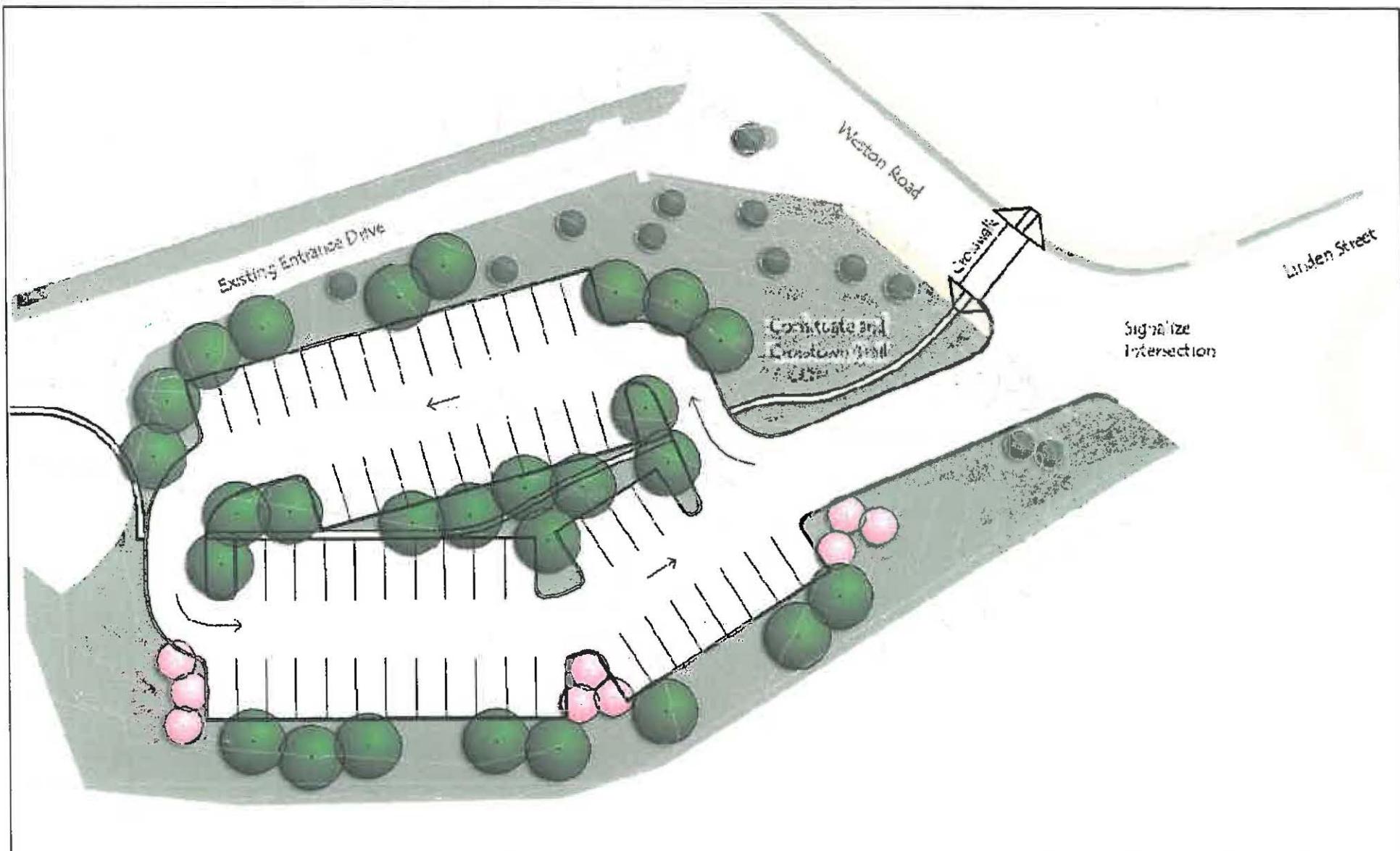
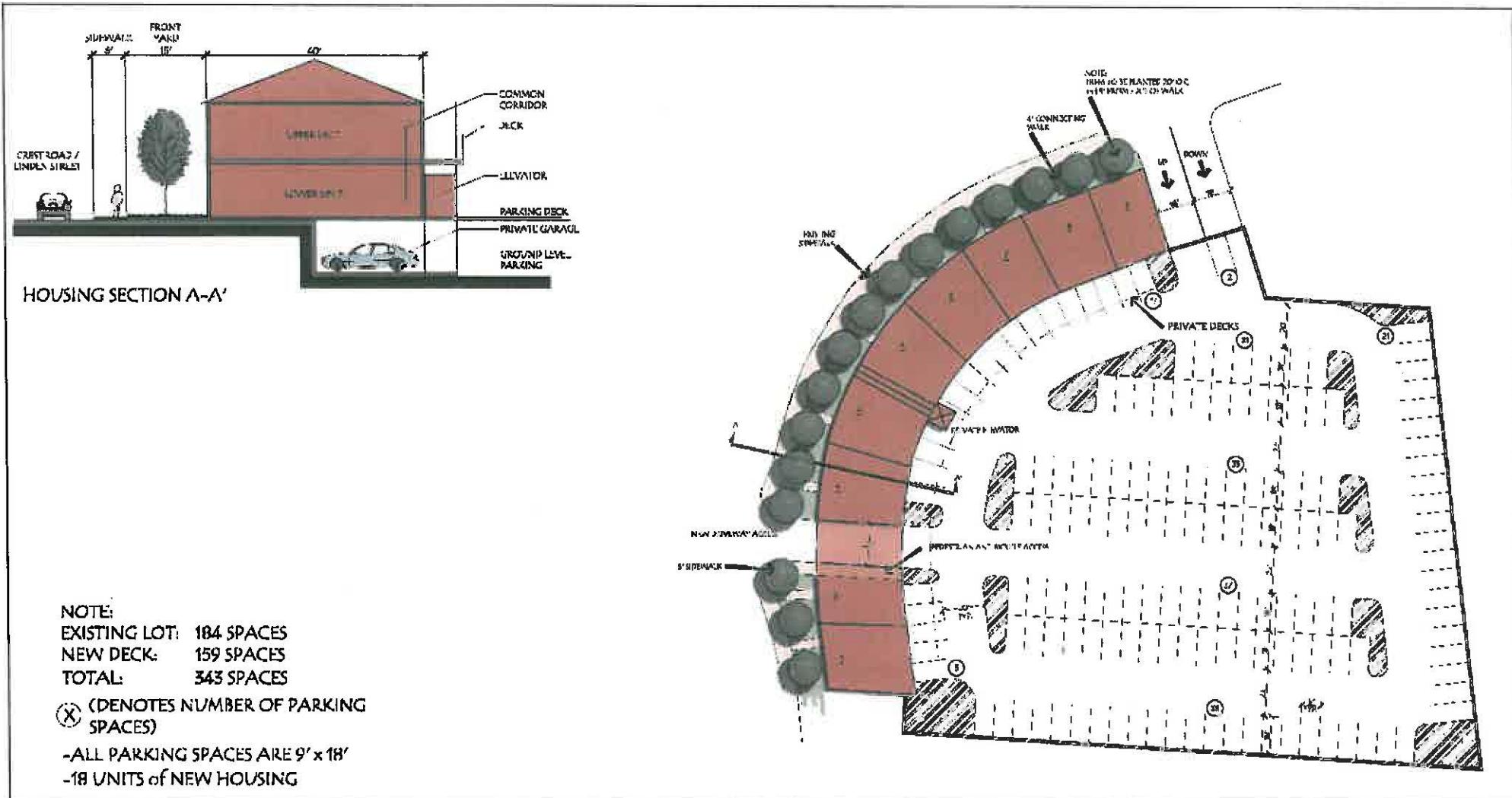


Figure 8: Weston Road Substation  
Conceptual Parking Improvement Plan



Wellesley, Massachusetts



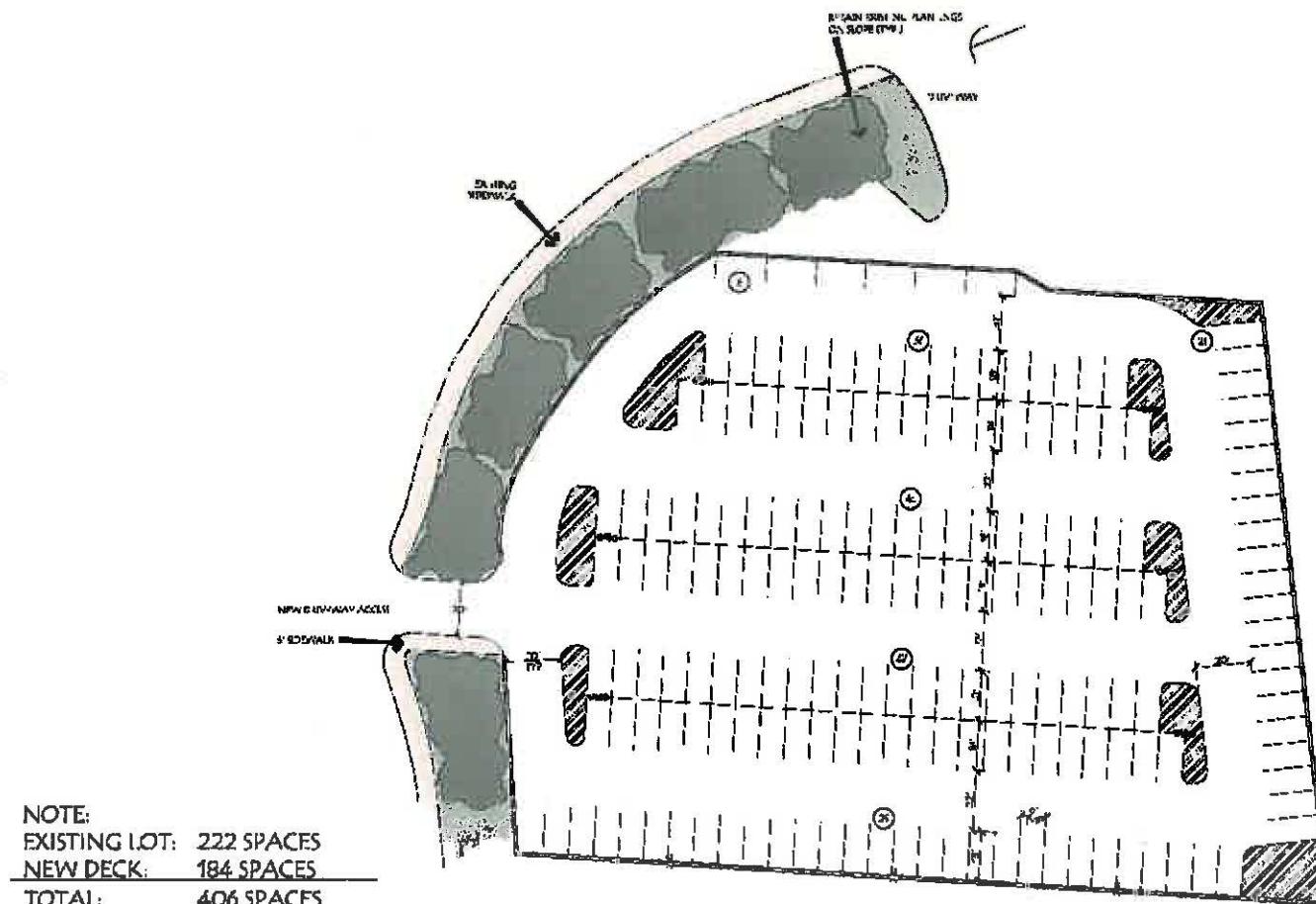


## Figure 9A: Tailby Parking Lot Conceptual Improvement Plan - Alternative 1



**Wellesley, Massachusetts**





**Figure 9B: Tailby Parking Lot Conceptual  
 Improvement Plan - Alternative 2**

Wellesley, Massachusetts



**Street Lighting** is proposed that matches the lighting installed at Wellesley Square. These fixtures are historic in character and are at more of a pedestrian scale than the current cobra head street lighting. The poles and bases should be located within and at the edge of the landscape planters so as not to impinge on the sidewalk width and for the maintenance of the sidewalks.

**Lighted Bollards** are proposed for the crosswalk neckdowns. These will strengthen the pedestrian crossings making them safer and will provide an attractive accent to the landscaping.

**Benches** are proposed at a new pedestrian node at the central crosswalk. This node is located on private property in order to provide a buffer between the street and people using the benches. Experience shows that people are more likely to take advantage of a street side resting area if it is set back from the traffic. A bench style should be selected that coordinates with the light fixture. We recommend eight-foot long iron benches with wood or recycled plastic slats.

**A drinking fountain** and two **trash receptacles** are located at the central pedestrian node. These likewise should be selected to coordinate aesthetically with the light fixtures and benches.

**Plantings** are recommended in several areas to soften the hard surfaces of the building and parking areas and to provide a more beautiful and comfortable areas for workers and shoppers. The primary **corridor street tree** is and should continue to be Lindens. They are beautiful and do well in a hot urban environment. The linear planting areas between the parking lots and the street contain the **corridor street trees** as well as shrub plantings. The plan proposes expanding the planter on the south side from six feet to nine feet in width to provide a stronger edge and a healthier growing environment for trees and shrubs. The planters should be planted with a mix of **deciduous and evergreen shrubs** that are maintained at a height not to exceed three feet. This will provide a buffer from cars on the street and in the parking lots but not obstruct views to the businesses. **Seasonal flowers** should be planted near driveway entrances and at the pedestrian node to provide color and variety. To assure healthy plantings the planters should be provided with **irrigation**.

New plantings are shown along the pedestrian corridor through the parking lots and at several places where there is room to expand planting. Breaking the large expanses of pavement with trees will make the area more attractive and moderate the heat of the pavement. Callary Pear trees are recommended for the pedestrian corridor due to their spring flowering and their ability to thrive in tough urban environments. Ground covers should be planted beneath the pear trees.

Diehl's maintains the planter on the north side of the street while the Town maintains the south planter. It is recommended that a private landscape company maintain the planters along the street and the plantings in the parking lots. This will assure better and more consistent **maintenance**. Perhaps the Linden Street Association of Businesses would be willing to fund maintenance of the planted areas. It is also recommended that a

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partnership between Wellesley Garden Club and Linden Street Association of Businesses is established.

The recommended improvements to sidewalks, lighting, street furniture, and plantings will provide a significant enhancement to the beauty and comfort of the Linden Street commercial area. These amenities will make the area a better one in which to work and to shop, supporting the enhanced economic environment of the area.

## Section 3

# Future Land Use Scenarios & Development Policies

Several meetings with primary land and business owners, Town staff and the Planning Board were held to discuss future development opportunities and necessary land use regulatory changes. It became evident after a review of the Linden Street Vision Plan, these stakeholder meetings, and public workshops that major amendments to the existing Wellesley zoning regulations in the would be difficult and contentious.

Most participants feel that the existing zoning is serving the area adequately. However, a number of permitted uses allowed by the Zoning Bylaws particularly in the Industrial District would not be desirable by town officials, area residents, or most of the business and property owners.

Many Linden Street stakeholders are looking for alternatives to the auto orientation of Linden Street. It is clear that an improved pedestrian environment is sought as well as a more traditional community atmosphere, which requires change beyond the public streetscape improvements recommended in Section 3 of this plan. Stakeholders also mentioned the desire for a better mix of uses such as more restaurants and small retail shops. The area also lends itself to certain housing types such as townhouses, condominiums, and market rate and affordable apartments, possibly in combination with office and retail uses.

Mixed uses are possible in the commercial section of the corridor given the area's proximity to established commercial districts, civic uses, schools, and MBTA commuter rail service. The single-use aspect of the commercial section of Linden Street has enormous market potential as a place for residents to live, work, shop and play. Linden Street can better incorporate the needs of current residents and customers more readily through the following opportunities:

- By maintaining and enhancing convenient parking for customers and residents;
- Through design guidance, creating attractive architecture and signage;
- Redesigning Linden Street and interior connections to balance and fully accommodate the needs of motorists, pedestrians and bicyclists;
- Providing full building accessibility; and
- Improving pedestrian access and use of public spaces.

This section of the Plan takes into account the data collected from previous plans, studies and inventories to determine growth patterns and its impact on the character of the Linden Street corridor. The Ten Year Potential Development and Redevelopment Scenarios in the Linden Street Corridor (Figure 10) provides a pedestrian oriented, mixed use vision of growth over the next 10-20 years.

### Land Use & Zoning Recommendations from the Vision Plan

Based on the results of the *Linden Street Vision Plan: Maintaining Character and Managing Change* (December 2000, Beals and Thomas, Inc.) several observations and recommendations to address land use and zoning issues were made to guide the current planning effort:

#### Land Use & Zoning Recommendations from the Vision Plan

- Businesses and residents indicated a preference for the existing land uses and want to focus on small aesthetic improvements;
- Public participation indicated that current zoning should remain as is except for 151 Linden Street;
- Phase II should study mechanisms for maintaining character though new development, and avoid the expansion of non-conforming uses.
- Possibly consider an overlay district that would control new uses while protecting existing ones; and
- The Diehl Family has owned most of the commercial property for 125 years and has been good property managers responding to community needs and desires. They have no intentions of moving but redevelopment of other properties could significantly effect the composition of the district.

### Current Land Use Patterns & Characteristics

Linden Street is a corridor with thriving local businesses and high value multi and single-family homes. It is bound on the south by the railroad and on the north side by residential neighborhoods.

The study area includes about 46.6 acres of land. There are about 81 residences and 167 businesses in the area. Linden Street is zoned about one-third industrial, one-third business and one-third residential but the existing land uses are about half commercial and half residential. The current zoning reflects the corridor's original purpose as rail yard and associated industrial uses. There is currently little industrial uses left in the corridor and new ones would probably not fit well with the current pattern of business and residential uses.

Commercial anchors in the area include Diehl's Hardware and Building Supply, Roche Brothers Supermarket, Captain Marden's Seafood and the Wellesley Volkswagen dealership.

The Diehl Company owns the majority of commercial property and buildings within the district including Roche Brothers Supermarket, Wellesley Volkswagen, and multiple retail spaces.

Table 7 illustrates the existing land uses and development patterns in the commercial core of the Linden Street Corridor between Kingsbury Street and Crest Road. This area encompasses approximately 24.4 acres. There are 25 buildings in this area containing about 228,642 gross square feet.

According to the current zoning bylaws, there are few new development opportunities remaining in this area of the corridor. Given the requirements for frontage, setbacks, on-site parking, open space, and floor area ratio (FAR) most properties could not be redeveloped or expand without a series of variances from the Town. For example, the commercial area has an estimated 1,159 total parking spaces (including the Tailby Parking Lot) while the zoning bylaw requires 1,310 spaces for the existing development. The existing floor area ratios on a number of properties also exceed the maximum permitted by the zoning bylaws (.3). These requirements are particularly constraining for some of the older properties in the area such as 151 Linden Street. This limited flexibility under the current zoning makes it difficult to achieve high quality, pedestrian scale redevelopment. It may also be hampering private investment that might be made in the corridor.



**151 Linden Street Building**

### Reinventing Strip Development

The Town of Wellesley and private property and business owners along Linden Street must anticipate changes in consumer trends as the community grows and develops. National preferences have changed over the past several years making older downtowns and traditional neighborhood development (TND) increasingly popular. Wellesley Square's renewed popularity and recent private investment is a good example of this. The characteristics that today's consumers are looking for are attractive, pedestrian friendly, and safe environments for living, working, shopping and entertainment. The Town of Wellesley, working in close communication with Linden Street property owners, business owners and residents, must provide the flexibility to adjust to these market demands.

Public commitment and consistency will encourage private investment. The Linden Street streetscape plan, traffic and parking improvement plans, design guidelines and regulation amendments are all important publicly-sponsored measures to implement the desired changes to Linden Street. The following corridor-wide objectives are critical:

- Integrate public facilities into the improvement strategy in a way that helps to shape the desired enhancements and investment by the private sector.
- Devise land use regulations that facilitate private implementation of the public strategy.
- Incorporate specific performance standards into zoning regulations for landscaping, signage, architectural quality, pedestrian linkages, and other planning details crucial to the overall success of the corridor.
- Coordinate and expedite the public review and approval process with limited jurisdiction.
- Direct public investments into the corridor such as infrastructure, streetscape improvements, housing and parking facilities to facilitate mixed-use and high quality infill development by the private sector.

In order for the commercial section of the corridor to be successful in the future it should look less like strip development. Some key public and private redevelopment strategies are as follows:

- High quality building design and retail-oriented first floor facades;
- Enhance and expand pedestrian-scaled features such as streetlights, sidewalk pavers, mature trees, quality signage, and landscaping; and
- Work with the local utility company to bury power lines.

### Conceptual Future Land Use Scenarios

Figure 10: The 10-Year Potential Development and Redevelopment Scenarios illustrate a series of private and public investments in the commercial core of the Linden Street Corridor that may occur over the next ten (10) years. These scenarios are based primarily on existing property use data and trends, land constraints, discussions with various stakeholders, proposed development standards, and existing market conditions in the project area. Together with the Linden Street Corridor Improvement Plan, these scenarios provide an idea of what the Linden Street Corridor could look like in the future. While the Recommended Improvement Plan and Future Scenarios are not regulatory instruments nor compel private property owners to follow possible development patterns, they are important long-range views of the area and its future.

Table 7 identifies areas of the commercial core that have the most development and redevelopment potential as A through F, and identifies the potential development scenarios including the following:

- New buildings and structures;
- Renovations and additions to existing building and structures;

- Net changes in building gross square footage; and
- Changes in the public and private supply of parking.

If all of the potential development and redevelopment scenarios occurred over the next 10 years it could result in the addition of four new buildings, the renovation or addition to eight existing buildings, and two new parking structures. The total new building under all development scenarios amounts to just under 72,000 square feet.

One of the primary concerns of property owners in the corridor is to expand parking opportunities where possible. Under the potential development scenarios approximately 343 spaces could be added to various sections of the corridor between Kingsbury Street and Crest Road. These additional spaces would primarily result from decking the Tailby Lot, a new parking structure behind 141 Linden Street, and reconfiguring existing parking lots to use space more efficiently.



**Tailby Parking Lot Showing Grade Difference**

Where appropriate, mixed uses are illustrated in future scenarios for the corridor. Buildings are two and three stories in height and brought to the street edge to create a friendlier pedestrian environment and more traditional downtown character.

### Facilitating Change in the Corridor

To facilitate potential development that creates a more traditional pattern of a walkable, mixed-use district, selected revisions to the existing zoning bylaws have been recommended. These are primarily in the areas of site plan review, dimensional requirements, signage, parking, and landscaping requirements (See Section 2). Alternatively, a new bylaw, introducing an overlay district to the Limited Residential, Business and Industrial districts within the corridor, could be adopted. The new overlay district would accomplish the mixed-use objective without changing existing underlying zoning regulations.

Unlike conventional ordinances, the Traditional Neighborhood District Ordinance can encourage historic development patterns and counter the visual effect of strip development. However, this proposed overlay district does not replace the existing base zoning districts. Property owners are not compelled to follow the provisions but are provided with a flexible set of alternative development standards that allows for more efficient land use and high quality development. Based on input from town staff, the Planning Board, property and business owners, and general public, it has been determined that the Traditional Neighborhood District fits the scale and desired development patterns of the commercial core area of Linden Street. The ordinance is designed to accomplish the following goals:

- Encourage social, educational, employment, recreational and shopping opportunities in close proximity to residences;
- Design streets and parking area to balance the needs of both automobiles and pedestrians;
- Provide for building sizes and character that define streets and public spaces;
- Reduce the requirements for infrastructure, automobile use, and pollution;
- Generate new private investment on the corridor.

Proposed By-law  
Linden Street Traditional Neighborhood District

## **1. Purpose**

In a manner consistent with the Wellesley Comprehensive Plan, the purpose of this Overlay District by-law is to maintain and enhance the aesthetic character, economic vitality, creative and efficient use of available space, and community-oriented, mixed-use environment of the Linden Street District. This district incorporates the areas zoned Limited Residential (LR), Industrial (I), and Business (B), which are fully served by municipal utilities. A mixture of uses within the overlay district and within lots and buildings is encouraged in order to accommodate a wide variety of users and to maintain and enhance the district's neighborhood character. All efforts should be made to create a pedestrian-oriented environment within the district by creating links between existing and proposed areas of activity in order to better serve the needs of residents and visitors. Accommodation of pedestrians, bicyclists and public transit should be evenly balanced with the needs for motorized vehicles. Given the availability and distribution of public and private parking facilities in the district and the desire to maximize the utility of scarce land resources, the creative use of off-street parking should be encouraged to provide for infill development wherever feasible.

## **2. Relationship to Other Bylaws**

Due to the critical importance of this Overlay District to long-term vitality of the district, this by-law shall be used and interpreted primarily on its own, and as an alternative to the underlying zoning districts. Certain other general sections of the bylaw will still apply to this District including but not limited to site plan review, design guidelines, signage requirements, and landscaping.

## **3. Permitted Uses**

Table 1 established the uses in the district that are permitted by right (P), by special permit (SP), and not permitted (N) in the Overlay District. Permitted uses are also allowed based on street level and orientation of use.

**TABLE 1: LINDEN STREET TRADITIONAL NEIGHBORHOOD DISTRICT USES**

<u>USES</u>	GROUND FLOOR	2 <sup>ND</sup> FL. & ABOVE
<b>Residential</b>		
Townhouses, Residential Condominium, apartment building	SP	P
Mixed residential/work space which Conforms to all building and life safety codes	N	P
Bed and Breakfast	P	P
Apartment Unit, Accessory Apartment (Studio apartments shall be a minimum of 600 SF; 1-bedroom min. of 750 SF; 2-bedroom min. of 1,000 SF)	N	P
Home Occupation	P	P
Group Home, Assisted Living Residence, Shelter, Halfway House, Single Room Occupancy	N	N
<b>Commercial</b>		
Miscellaneous professional and business offices and services including, but not limited to legal, and other professional services and finance, banking, insurance and real estate offices	P	P
Medical Center including accessory medical research and associated facilities	SP	SP
Business offices including such uses as corporate offices, back offices, and insurance processing but excluding professional services, such as real estate, banking or legal, or any office where the primary function is to provide services to retail customers or individuals	SP	P
Retail establishment selling principally convenience goods including, but not limited to, food, drugs, and proprietary goods and establishments selling general merchandise, including, but not limited to, dry goods, apparel and accessories.	P	P
Outdoor Entertainment (Concert and Festival areas)	P	P
Bank	P	P
Bank Drive Thru	SP	N
Personal and Consumer Service Establishments	P	P

Restaurant and pubs where consumption is primarily intended to be on site	P	P
Outside Accessory Retail and Restaurant Uses (i.e. sidewalk dining areas)	P	P
Research and Development Facility	SP	P
Delivery Services	N	N
Funeral home	N	N
Medical or Dental Office	SP	P
Hotel/Motel	P	P
Pool or billiards hall, amusement arcades, and bowling alley	SP	SP
Movie Picture and Live Theater not including night club/dance hall or adult uses	P	P
Repair Service Establishment	P	P
Library/Museum	P	P
Trade/professional school	SP	P
Private Club	SP	SP
Convention Center	SP	SP
Commercial and public parking lot or structure	SP	SP
Day Care, school aged child care program and family day care	SP	P
Membership Club for health or athletic facility operated for profit	P	P
Outdoor commercial recreation use, miniature golf, temporary carnivals	SP	N
Night Club/Dance Hall	SP	SP
Telecommunications Facilities, Structures and Antennas	N	N
Bus and Rail Passenger Terminals	SP	SP

Ground Floor Use – Those uses with primary access, frontage and orientation to Linden Street at ground level.

2<sup>nd</sup> Floor and Above – Those uses with primary access, frontage and orientation to Linden Street but separated from ground level by one story or more.

P – Allowed by right. All uses must comply with all other applicable codes. (Site Plan review may be required for uses above a certain threshold)

SP – Allowed by Special Permit

N – Not permitted in the District

Commentary: Important to the long-term viability of the Linden Street commercial area is allowing for the expansion of a broad and flexible range of commercial, professional and residential uses. A combination of uses including retail, food and entertainment, professional services, recreational, cultural, and governmental must be permitted if Linden Street is to remain as a "place of necessity" for residents and continue to attract prospective investors. Well placed professional offices and residential uses (optimally on upper floors) fill vacant/underutilized spaces, create investment opportunities, provide built in security, and reduce traffic (i.e. you can

live, work, shop and eat all within walking distance). Only truly incompatible uses should be separated in the overlay district. Much of the potential for incompatible uses can be addressed through performance standards and vertical separation, which ensure that conflicts between certain uses don't occur. This proposal allows for a broad range of uses and performance standards where certain uses could create conflicts under certain conditions.

#### 4. Use Dimensional Standards

Table 2: Dimensional Requirements in the Linden St. TN District						
Minimum Lot Requirements			Min./Max. Yard Setbacks			Max. Lot Coverage
Area (S.F.)	Frontage (FT)	Width (FT)	Front	Side	Rear	% of Lot
None	25	None	0/10	0/10	10/40**	100%
Building Height: 15 feet Minimum; 40 Feet Maximum***						

\* Maximum rear yard required when abutting residential district

\*\* A 40-foot rear setback is required only when use abuts GR District

\*\*\* Maximum building height is 40 feet or 3 stories. Additional height and stories may be permitted by special permit from the ZBA

Commentary: Linden Street's scale and layout should be distinct, and the overlay district ensures that: new development is built with pedestrians in mind (close or up to the sidewalk and adjacent buildings), no parking is allowed within the front-yard setback but behind or on the side of the building, and landscaping and streetscape amenities are provided (benches, hard surfaces, street trees, etc). Sufficient height must be allowed by right to permit new investment and a necessary mix of uses (particularly professional and residential) to create economic opportunity and reduce travel needs.

#### 5. Performance Standards and Additional Requirements

5.1 **General Requirements** - In any permit proceeding (i.e. site plan review, special permits, design review), it must be demonstrated that relevant goals in the adopted purpose statement of the Linden Street TN District are satisfied. Toward that end, consideration shall be given to possibilities for improvements to pedestrian and vehicular circulation. At a minimum, the applicant shall propose alternatives for closing, sharing, or consolidating curb cuts, creating easements and links with adjoining uses or properties, moving parking areas to rear yards, merging parking areas to create more effective and efficient use of land, and upgraded sidewalks, paths, and crosswalks.

**5.2 Use of Existing Buildings** – The full use of buildings existing on the date of adoption of this section is permitted in accordance with underlying zoning districts and applicable requirements. In accordance with the District, full use of first floors may be permitted on an unrestricted basis for all uses permitted in the District. Full use of upper floors may be permitted on an unrestricted basis for non-retail uses. Retail uses may be permitted on upper floors only if all bylaw requirements are fully satisfied on the ground floor.

**5.3 Outdoor Uses** – Outdoor uses are encouraged where applicable as accessory uses to retail, restaurant and entertainment uses. Outdoor dining is permitted in accordance with the provisions of the Wellesley General Codes. Accessory and temporary outdoor retail, amusement and entertainment uses are permitted in accordance with Section 3 of this bylaw.

*Commentary:* Expanded outdoor uses on private and public property are very important to attracting residents and customers to Linden Street. A new Outdoor Dining Ordinance should be considered that would allow for more outdoor entertainment, and food uses along Linden Street.

**5.4 Design Guidelines** – All new construction, expansion, façade alterations and other building or site improvements should be in character with the Wellesley Design Guidelines. New construction and alteration of existing historic buildings should preserve/employ architectural elements and building materials as recommended in this section of the Zoning Bylaws. In any permit proceeding, consideration shall be given to how a project maintains or enhances the aesthetic character of this District, in a manner consistent with the Design Guidelines. The applicant is required to demonstrate that proposed renovations, additions, or structures will be compatible with, and will enhance, the flavor and character of the Linden Street Area.

**5.5 Applicability of Setbacks** – The use of setbacks for front and side yards as prescribed in Table 2 above will only be permitted when the area is used for pedestrian access, outdoor accessory uses, or to facilitate access to the rear of the lot for parking and loading. No parking is permitted in the front yard setback. Streetscape plans shall be provided demonstrating that the setback area accomplishes these objectives and creates an inviting environment for pedestrians (i.e. concrete/brick pavers, designated dining or retail areas, street trees and furniture, decorative lighting consistent with equipment used by the Town). Where rear yard setbacks are required, site plans will demonstrate that appropriate screening is provided (i.e. trees and fencing).

**5.6 Building Height and Bulk** – New buildings should be constructed to a height of two or more stories and as close to the street as possible in close proximity to adjoining buildings with effective pedestrian accommodations. New single story building and alteration of existing single story buildings (excluding change of use) in the district are only permitted by Special Permit. Where permitted, new and altered single story buildings must provide façade improvements and front

elevations that are at least fifteen (15) feet in height above street elevation and constructed with materials and styles consistent with the Design Guidelines. Taller buildings than prescribed in Table 2 above may be allowed by Special Permit if such additional height is consistent with the scale of surrounding buildings, provides economic and civic benefits to the Town and surrounding district, and is consistent with the Design Guidelines.

5.7 **Streetscape Design & Pedestrian Amenities** - In any permit proceeding, consideration shall be given to possibilities for enhancement of and improvements to streetscape design and pedestrian amenities. At a minimum, the applicant shall propose a streetscape design that may include, but is not limited to: planting of street trees; landscaped areas; park benches, sidewalks or other pedestrian paths; doorways, porches, and entries that provide transition for and bridge the gap between public and private space; and carriage-type street or building lighting that is appropriate in style and design to desired character of the District. Wherever feasible, power lines shall be buried, or moved behind buildings.

5.8 **Storage Areas** – Exposed storage areas, machinery, garbage “dumpsters,” service areas, truck loading areas, utility buildings and structures shall be screened from view of abutting properties and streets using plantings, fences and other methods compatible with the goals of this section. Where feasible, shared use and designated areas for garbage dumpsters shall be required.

5.9 **Temporary Street Banners** – Temporary street banners used for the purpose of informing the general public of community events and activities may be permitted with approval of the Board of Selectmen. Street banners shall be hung in prescribed locations, securely fastened to supporting structures, maintain a minimum height of 16 feet above the street, be constructed of durable materials, used solely for community events in the district, and remain in place for no more than three (3) weeks prior to the event.

## 6. Off-Street Parking and Loading Standards

The regulations with regard to off-street parking and loading prescribed under this Section shall supercede the requirements under Section XXI of the Zoning Bylaws.

6.1 **General Parking & Circulation** - In general, the applicant and the Town should seek to preserve and expand the supply of parking spaces. However, in certain cases, it may be preferable to shift and consolidate parking spaces to help achieve other goals related to streetscape design, district vitality, or public safety. Parking and circulation shall be designed to provide for the maximum pedestrian safety, ease in traffic flow, and access/egress on the property, while minimizing the need for impervious surfaces and maintaining the visual character of the property and adjacent areas.

6.2 **Parking Requirements** – Where on-site or controlled parking is necessary and required, the applicant shall provide an amount equal to seventy-five (75%) of the required spaces under Section 4.2 of the Zoning Bylaws in recognition of the

significant amount and distribution of public and private parking spaces available in the District. No additional parking shall be required for change of uses or the addition of accessory uses within existing buildings or on site.

**6.3 Public Parking Fund & Permit Program** - Parking requirements may be satisfied on-site as per this Section. Alternatively, where it is demonstrated that on-site parking is neither feasible nor desirable, a contribution in lieu of parking shall be made to the Municipal Parking Fund at a rate of \$500 per required parking space. The Municipal Parking Fund will be utilized exclusively for the enhancement and expansion of public parking facilities in the Linden Street Traditional Neighborhood District. For residential parking only, applicants may obtain public parking permits in designated municipal lots and spaces to satisfy on-site requirements. Municipal residential parking permits shall require annual proof of residency, prescribe specific hours and location of use, and an annual permit fee.

Commentary: A parking fund and permit program should be defined broadly so that it may be used for purposes reasonably related to creating or reconfiguring public parking spaces. Some examples may include closing or consolidating curb cuts, managing the supply of or demand for parking spaces, new or improved pedestrian paths or amenities, directional signage, and bicycle parking or public transit facilities, and other improvements that reduce the number and length of vehicle trips associated with parking needs.

**6.4 Off-Site Parking** – Parking requirements may be satisfied if an off-street municipal parking lot of 20 spaces or more exists within 600 feet of the proposed use. If this rule cannot be met, the applicant can secure private off-site parking within 600 feet of the site by ownership or lease with another property owner if it can be demonstrated that excess spaces are available.

**6.5 Shared Parking** – Where possible, shared parking is strongly encouraged and the required number of spaces may be reduced if adjoining uses are compatible and can demonstrate that such a reduction would still provide adequate parking.

**6.6 Access and Loading** – New curb cuts on existing public ways shall be minimized. To the extent feasible, access to businesses for purposes of delivery or parking shall be provided through one of the following methods: (a) through a common driveway serving adjacent lots or premises; (b) through existing side or rear streets and access points thus avoiding the principal thoroughfare.

**Commentary:** There are approximately 1,069 well-distributed parking spaces in the Linden Street Traditional Neighborhood District (208 public spaces and 861 private spaces). However, the existing parking requirements for the underlying zoning districts are quite stringent. (A survey of peak turnover and occupancy rates should be carried out). Additionally, there are a number of areas where private parking is underutilized and could generate new spaces with minimal disruption. This proposed District allows for private parking requirements to be fulfilled by public parking within a giving distance or agreements for use of existing nearby private spaces. Provisions are also made for shared parking in compatible situations (i.e. professional office and residential).

## Section 4

# Implementation Program & Action Plan

The Implementation & Action Plan includes a list and schedule of projects, policies and actions necessary to achieve the goals and objectives of the Linden Street Area Master Plan over the next 10 years. (See Table 8 below).

Prioritized actions and target dates are based on the recommendations of the Planning Board and input through the public workshops. Concept level cost estimates for projects as well as potential funding sources are also identified.

It is recommended that a subcommittee be established by the Board of Selectmen to oversee the implementation progress and incorporation of the plan into other town policies such as the capital improvement plan, zoning and subdivision regulations, and budget. An annual work program should be developed and carried out by the subcommittee with the assistance of various town departments. Progress reports should be made biannually to the full Planning Board, Selectmen, and general public. As certain conditions change so may priorities. The Action Plan should be reviewed annually and modifications made according to changes in priorities.

## **Appendices**

- Appendix 1:** Minutes of Public Workshop #1 – Public Safety and Vehicular/Pedestrian Circulation
- Appendix 2:** Minutes of Public Workshop #2 - Design Guidelines, Public Improvements & Parking Issues
- Appendix 3:** Minutes of Public Workshop #3 – Land Use, Zoning and Development Standards
- Appendix 4:** Project Participants

February 5<sup>th</sup>, 2002 Great Hall  
Linden Street Steering Committee  
Workshop #1 --- Public Safety and Vehicular / Pedestrian Circulation

## SUMMARY

- 7:30pm. Introduction and history of Linden Street Study project by Rose Mary Donahue, Chairmen of the Wellesley Planning Board. Introduced Lance D. Hansen as newest member of the Planning Department and planner for facilitating the final phase of the Linden Street study.
- Review outline of scope of service and schedule of future workshop sessions
- Introduction of Ted Brovitz, Project Manager and Peter Jackson, Senior Landscape Architect from the consulting firm of Dufresne-Henry Inc., Boston
- Dufresne-Henry presented workshop topics for discussion of vehicular and pedestrian circulation issues:

### Issues discussed:

1. speed and volume of traffic: Linden Street used as cut through to avoid Central Square traffic. Noticed at intersection of Weston Rd, in the afternoon traffic backs up because of cars turning left. Also noticed, pedestrian difficulty to cross Linden Street during early afternoon. Intersection of Kingsbury Street has limited sight distances.
2. Police data for recorded accidents: May 99 Jan 02 --- 40 reported accidents on Linden Street public way - three from Weston Road to Crest Road, six from Crest Road to Everett Street. 17 from Everett to Pine Tree Road. 14 accidents reported from Pine Tree Road to Kingsbury Street. 24 of the 40 were rear end accidents. Indicate excessive speeds and left turn movement in front of on coming traffic. Not really an excessive number of accidents, but want to make the area safe as possible. Discussion of the signalized intersections at Crest Road and Everett Street intersections. Additional signals may cut down the number of vehicles for thru traffic.
3. Curb cuts (number and alignment). Close together and they do not align across the road from each other, making it difficult for pedestrians to move across either sides of Linden Street when shopping.
4. Pedestrian area: confusion and unclear. Confusing pedestrian routes. Worn out brick paving. Crosswalks not as strong a design. Only painted on the surface. High number of jay-walking occurrences. Sidewalks are 8 feet wide on the northern side and 5 feet wide on the southern side. The five foot width is too narrow while 8 seems adequate.

Speed: Do not recall any speed signs on Linden Street. Discussions about the need to post speed limit at strategic points. Mix of local business and through traffic accounts for high number of rear end traffic accidents. Shared corridor problem. The real issue is design. Can post speeds but if the design allows for high speeds, that is what you will get.

- Pedestrian movements: no pedestrian paths from sidewalk to store front through parking lots. Goal: provide design solutions without reducing the number of available parking spaces. Janie Batista commented the need to delineate pedestrian age groups. Middle school students move faster and in groups of people. Elders walking are slower moving.
- The feasibility of a pedestrian crossing over the railroad tracks. Issues of public safety. Have identified two high level of crossing locations: 1.) proximity of Wellesley Volkswagen and elderly housing, 2.) the other near the Wellesley Square Post Office. Bridge would need required high clearance for trains, and be handicapped accessible. Economics feasibility will be further researched.

Traffic calming:

1. Traffic pedestrian lights. Impact of two intersection signals.
2. Narrow travel lane, can be accomplished without redesign road width.
3. Narrow roadway.
4. On street parking.
5. Strong pedestrian crossings.
6. Orderly curb cuts.
7. Stronger directional signage to help motorists make safe driving decisions.

Thomas Frisardi asked about any accident data for Central Street.

Edwin Silver noted that even when at off peak, traffic flow is fast on Central Street

Richard Sarginson also noted that even with on street parking, there are a high number of accidents in the Lower Falls area.

Jeanne Lukas commented that by narrowing and slowing down traffic on Linden Street, cars will find other alternatives choosing to go through residential neighborhoods.

Richard Sarginson noted the trade off between traffic calming and traffic congestion.

Steve Fader asked the consultants about the idea of pedestrian signals: possibility of using the pedestrian actuated flashing yellow pedestrian warning signals. Seem to be working well for other areas in town. Police Deputy Bill Brooks explained that those lights do not require you to stop but alerts the motorist that they are traversing a pedestrian area.

\*\*\* Ted Brovitz example story of pedestrian flag waving in Salt Lake City.

- Crest Road: showed traffic neck down cutting one lane on westbound traffic. Neckdown will squeeze traffic at the intersection.

Walter Woods commented that remedy is worse than the problem at the Crest Road intersection. Felt that plans create reverse or double curves and have eliminated a by-pass for vehicles wanting to make a right hand turn. Also noted that 2 years ago, intersection improvements were presented at town meeting and people voiced strong opposition.

- Pedestrian Environment:
  - Improve sidewalks
  - Stronger crosswalks
  - Pedestrian links through parking lots
  - Shorten crosswalks
  - Enhance landscaping

Shannon Scarlett raised the issue of the pedestrians crossing the driveway entrances. Question asked about possibility for barrier between pedestrian and vehicular traffic.

- Circulation Concepts
  - Strong pedestrian circulation along buildings and across parking lots
  - Alignment of curb-cuts and crosswalks

Peter Jackson noted that existing roadway dimensions of Linden Street are fit for high travel highways. Existing roadway conditions (moving south to north):

6' planter owned 1/2 by town... 1/2 by F. Diehl & Son, Inc.  
 5' width sidewalk  
 34' width of roadway (13'-6" travel lanes)  
 8' sidewalk with street lamps  
 11' width planter owned and maintained by F. Diehl & Son, Inc.

- Presentation of the traffic calming concepts:
  - Alternative 1.) existing pavement with inclusion of bicycle lanes  
 (11' travel lanes --- 6' on-street bicycle lanes)
  - Alternative 2.) addition of alternate on-street parking one side  
 (11'-6" travel lanes --- 8' width of parking)
  - Alternative 3.) expansion of sidewalks  
 (8' width of sidewalks --- 10'6" travel lanes)
  - Alternative 4.) addition of on-street parking both sides  
 (11'-6" travel lanes --- 8' width parking lanes)
  - Alternative 5.) planted median --- sidewalks relocation  
 (11' travel lanes, 10' planted median, 6' internal sidewalks)

## Linden Street Project Study Area Map

Indication of existing amount and the location of curb cuts --- overlay conceptual diagram of potential traffic and pedestrian circulation. Initial reaction comments by the Steering Committee that by reducing the number of curb cuts and by reducing the width of the street, it will make it less desirable for motorists and consumers to travel on Linden Street. In the 1950's business owners wanted additional traffic passing their store fronts. Peter Jackson noted that the Linden Street businesses are destinations and traffic passing by using Linden Street as a cut through, are not stopping to shop at the local businesses. Also mentioned that having numerous curb cuts take away potential parking spaces.

Jeanne Lukas asked: "Are you considering reducing the total number of curb cuts. Do not, as this would create traffic congestion, making it difficult for people to enter /exit parking lots." Beth Murphy asked: "Will the opportunity for drive through windows be lost?" Peter Jackson responded that specific curb cuts have not yet been addressed and will be further looked into for the next workshop.

Walter Woods also noted that by reducing the number of curb cuts it will create anxiety, decreased patience and therefore more accidents from waiting to exit or enter the business parking lots.

Lewis Collins asked more about the inclusion of bicycle lanes along Linden Street. Peter Jackson said that there is not a lot of bicycle traffic currently using Linden Street, but another alternative option to designated bicycle lanes on Linden Street, might be in possibly utilizing part of the MBTA railroad excess land right-of-way, parallel to Linden Street for a "trail-with-rail" opportunity. More about this concept will be discussed in future workshops.

Richard Sarginson also mentioned about the potential for on-street parking, and felt that it is too remote from the businesses. The people would not park on-street except when there is congestion inside the parking lots. Shannon Scarlett also noted that if on-street parking were available it might provide additional spaces for commuter traffic.

Tom Fontaine stated that if curb cuts are located across from each other, it may create a more dangerous situation because it will encourage people to cut in front of traffic to quickly get from one parking lot to the other.

Sarah Murphy noted that currently customers will drive from business to business.

Janie Batista sited that she will park at one spot and then walk around because the parking lots are always full and if she leaves, there is no guarantee of getting another parking space.

Dean Behrend referred to the reported number of accidents between Crest Road and Kingsbury Street, and offered that the new intersection signal lights may slow down the traffic. Also noted that on the map there are five or six curb cuts on the northern side of the road and some could be widened and have four lanes of moving traffic, for left and right hand turning.

Peter Jackson described a traffic calming measure of narrowing the roadway near pedestrian crossings. This would slow down traffic, and use landscaping to create a pedestrian zone. Introduction of the "Pedestrian Corridor through Parking Lots" diagram.

Peter Jackson also offered a new parking layout for the northern side of Linden Street that would maximize the amount of parking space opportunities. Sarah Murphy indicated that the spaces at Roche Bros are too tight together, which leads to a higher accident rate.

Susan Littlefield also noted that snow storage becomes an issue for parking lots during tough winters with created snow banks.

It was also noted that SUVs cause traffic congestion and accidents as they do not see where they are going or when they are pulling out of a parking space.

Joe Grignaffini recommended that he would rather see Linden Street Commercial Area become narrow with more landscaping, than to have on-street parking. Also felt that the on-street parking was not aesthetically pleasing.

Dean Behrend sited a parking lot design in Norwood with one way entry and exit and then nose to nose parking with clear directional circulation signs.

Peter Jackson indicated that currently there is a mix of parking designs: 90 degree and angled parking, which can very distracting and confusing for motorists.

Peter Jackson also noted that parking issues will be addressed at the next workshop meeting, while revisiting many of the presented alternatives of workshop #1.

Rose Mary Donahue explained the goal of the workshop was to present a whole array of improvement opportunities, available for the future renovations to Linden Street and seek the input of the residents to help make these decisions.

A date was set for the next Steering Committee workshop of Tuesday March 19<sup>th</sup> at a time, which will be determined.

Workshop meeting adjourned: 9:15pm.

LINDEN STREET PROJECT COMMITTEE

TUESDAY, MARCH 19, 2002,

7:15 P.M. GREAT HALL

TOWN HALL

MEETING NOTES

Present: Rose Mary Donahue, Patricia Grandieri, Bob Sechrest, Robert Dischino, Jeanne Lukas, Paul Cremonini, Christopher Clark, Beth Murphy, Greg LeBrun, Edwin Silver, Ralph DiGiandomencio, Jon Erickson, Richard Sarginson, Arshag Merguerian, Eugene Perdoni, Keith Marden, Valerie Martin, Ronald Labadini, Dennis Dischino, Sarah Murphy, Louis Petrucci, David Petrucci, Janie Batista, Joseph Grignaffini, Joe Feiner, Bill Hutchinson, Rick Brown, Ted Brovitz, Peter Jackson, and Lance Hansen.

Rose Mary Donahue opened the meeting and welcomed those in attendance.

Lance Hansen gave a background report and orientation to the topics at hand. He indicated that this is the second Linden Street workshop to focus on parking. The first workshop held on Tuesday February 5<sup>th</sup>, 2002 focused on traffic circulation. He emphasized that he has spent considerable time in one-on-one conversations with people in the area and invited continued input along this line. He said that this evening the workshop would focus on parking opportunities throughout the corridor. In working left to right through the corridor, several opportunities were identified. The 1999 proposed Weston Road Substation parking lot, the Tailby Parking Lot and parking throughout the balance of the core Linden Street commercial area are the main areas to be addressed at this workshop.

Peter Jackson discussed the Weston Road Substation lot. Drawings were not made available for this meeting, but noted that he wanted to gauge people's reactions for multiple use for utilizing the potential parking area. He spoke of the previous interest in providing parking spaces for employees of the Central Street businesses as well as the potential of additional parking for the residential condominium tenants in the area. He said the Weston Road/Linden Street intersection is presently at Level Of Service (LOS) F and would certainly need to be signalized if the access into the potential parking were to be relocated across from Linden Street. He also recognized the poor sight lines existing along Weston Road to the south and will further study the options for the driveway to serve the lot. He said it is important to determine beforehand the market for those parking spaces.

Mr. Jackson showed possible development plans for decking the Tailby Parking Lot. He said an advantage in decking is the existing grade difference between Crest Road and the lot surface. He said that the new signal to be installed this spring could perhaps include a phase in the future for a new secondary lot entrance for a potential decking structure. He

said there could be a total of 406 parking spaces with a newly created parking deck. Also shown was a schematic design for a parking deck including 18 housing units at the street level. He noted this would cut down on the number of total available parking spaces (343). He acknowledged that there are issues with commuters from out of town using spaces in the lot. Recommendations that this could be partly addressed by increasing the fee for commuter parking and by changing the parking in the Railroad Lot to all two hour parking. Mr. Jackson asked Mr. Hansen that a few questions could be taken now.

Ronald Labadini said that considerable time could be devoted to discussion of Weston Road and Tailby Parking Lots. He thought there should be a separate meeting specific for addressing these town-owned parcels of land. Peter Jackson stated that the intent of this study is to address these parking opportunities within the corridor, but suggested that further study would need to occur for follow up to the recommendations.

Deunis DiSchino said that the parking spaces in the Weston Road lot could be leased to Central Street Merchants. He also agreed that area condominium residents could benefit from the availability of the spaces for overnight parking, and stated that some of those residents do use the Tailby Parking Lot currently for overnight parking. He also commented that the proposed Weston Road lot would not work for shoppers because too remote from the businesses on Central Street. He felt that what is needed are small parking lots throughout the business area.

Edwin Silver underscored the parking need for Central Street. He applauded the effort to promote the Weston Road lot. He felt that the project to deck the Tailby Parking Lot would not move forward quickly since it has already been discussed for the past 30 years.

Robert DiSchino objected to consideration of housing at the Tailby Parking Lot. He said that a 15-foot street setback is not enough, since standards for other parts of town are 30-feet. He also expressed concerns about decking. He felt that the Weston Road lot would be more cost effective.

Jane Batista felt that the provision of parking at Weston Road alone will not be enough to satisfy the parking demand in the area. She felt that decking the Tailby Parking Lot will not be an eyesore because of the grade difference. Based on an informal study she was familiar with, more than half of the cars normally at Tailby are from out of town. She asked what portion of a decked Tailby would be for commuters. Mr. Jackson said that would need to be determined. He recommended that the deck could be funded in part by parking revenue if the funds are segregated. He said that the parking rate is only \$2 per day and felt could be raised to \$5, while still being affordable for commuter use.

Christopher Clark said that the Selectmen have asked the Town's traffic engineer to do a study of parking rates. He confirmed that the funds are segregated.

Mr. Silver commented that if parking is leased the rates have to be advantageous over other parking.

Mr. R. DiSchino said that some of the spaces could be roped off until after commuting hours. This is a no-cost solution that could be tried.

Arshag Merguerian felt that the housing alternative for Tailby is not desirable because there would be inadequate green space and it would cut down the amount of parking that could be realized.

Mr. Silver added that if the residents have visitors even more spaces would be lost.

Mr. Jackson explained the proposed changes in the Linden Street core commercial area. He said that a primary consideration in planning for this section of the street is pedestrian and traffic safety. Initially it was felt on-street parking could be provided along Linden Street for additional opportunities but this idea was felt to be inadvisable from the first workshop comments. A 29-foot wide roadway cross-section is suggested with eight-foot wide sidewalks on both sides and a wider planting strip on the southerly side. This will provide two 11-foot wide lanes. The effort is to reduce the visual width of the vehicle travel way to encourage traffic to slow down and reduce cut-through traffic.

Mr. Jackson mentioned the upcoming project to install traffic lights at two intersections Crest Road and Everett Street. He also noted that there are mature trees now along the roadway in the planters but more could be added. He referred to the plan showing the existing and proposed new trees.

There was considerable discussion of curb cuts both in terms of number and width. Several curb cuts are proposed for closure some others are proposed to be narrowed. He noted which curb cuts that the businesses are served by 18 wheel trucks. The turning templates will need to be applied to the curb cuts to insure that the trucks can negotiate the turns safely. The goal of the narrowing of certain other curb cuts is to improve safety. He said with the proposed changes for curb cuts, there would be an increase in the number of parking spaces available.

Mr. Jackson discussed the possible alternatives for the easterly end of the study area. The corridor plan showed the potential for 11 new on-street parking spaces located in front of the 277 and 279 Linden Street businesses. He also described the possibility of closing one of the entrances to 277 Linden Street and connecting the parking area of that building with the commercial building to the east, Captain Marden's Restaurant (279 Linden St).

Mr. Marden said that he also has 18 wheel trucks making deliveries and expressed concern about maneuverability in the event of modifications. In discussion of parking needs he said that he now leases spaces for his employees in the Town parking lot in back of the Needham Cooperative Bank on Washington Street. He said that his parking supply is adequate.

Ralph DiGiandomencio said that the Diehl's Oil business needs to have adequate access for the delivery trucks. He questioned the design shown on the plan which called for narrowing the curb cut at this location. Mr. Jackson acknowledged the point and said that the final drawing would make sure the width is adequate. Elizabeth Murphy said that similar trucks must access Roche Bros, Linden Texaco and the Diehl's lumberyard itself.

Consequently the curb cuts serving those businesses must be wide enough to accommodate the trucks.

There was some discussion of the easterly curb cut to the Diehl's property on the northerly side of Linden Street. Sarah Murphy noted that particular curb cut serves the businesses at that location and helps avoid "mixing" of the automotive traffic and the truck traffic.

Mr. Sarginson stated that one of the most heavily used roadway segments for pedestrians is the section of street from Jimmy's Café to Roche Bros. He felt that the island should be lengthened in that area to provide for a safer pedestrian area.

There was discussion of creating a safe pedestrian corridor from the sidewalk to the storefronts. Several committee members felt that is a particularly important design element. Mr. Jackson explained the plan showing how a pedestrian corridor perpendicular to the roadway can be established without the removal of the total number of parking spaces.

Mr. Sechrest asked about the insurance building at the corner of Linden Street and Kingsbury Street whether there is adequate parking. It was decided that parking modifications were limited in this area, and probably appropriate for the type of business located at the address.

Ms. Batista said that she favored the creation of a safer pedestrian corridor but said she worried about making a worse traffic situation and problems for delivery vehicles.

Dennis DiSchino warned against losing parking at 277 Linden Street. Mr. Jackson said that the intent was to add about 14 spaces (three spaces in the lot and 11 on the street). Mr. DiSchino felt that there is not enough room in the street now. He stated if anything more land should be given to the business side to pick up a few spaces along the inside edge of the sidewalk.

There being no further comments Mr. Jackson turned the meeting back to Mr. Hansen. Ted Brovitz from Dufresne-Henry was introduced for considerations for the final workshop planning items. Mr. Brovitz has had experience in other communities working with business organizations and has a business background. He introduced that the third workshop will focus on issues of land use, zoning opportunities and development standards. Mr. Hansen added that under consideration, a zoning overlay for this district to provide business owners more flexibility in terms of land use, floor area ratio, parking standards and signage.

All were thanked for their attendance.

Meeting adjourned 9:00 pm.

Submitted by:  
Rick Brown - Town Planner

LINDEN STREET PROJECT COMMITTEE

TUESDAY, MAY 21, 2002,

7:15 P.M. GREAT HALL

TOWN HALL

MEETING NOTES

Present: Rose Mary Donahue, James C. Meade, Patricia Grandieri, Bob Sechrest, Thomas Frisardi, Sue Wright, Jeanne Lukas, Paul Cremonini, Beth Murphy, Greg LeBrun, J. H. Lebow, Ralph DiGiandomencio, Richard Sarginson, Eugene Perdoni, Renso Perdoni, Valerie Martin, Ronald Labadini, Dennis Di Schino, Sarah Murphy, Ron Labadini, Louis Petrucci, David Petrucci, Joseph Grignaffini, Marc Gulkasian, Robert Di Schino, Rick Brown, Ted Brovitz, Peter Jackson, and Lance Hansen.

Chairman Donahue opened the meeting and welcomed those in attendance.

Mr. Hansen gave a background report and orientation to the topics at hand. He distributed a 23-page draft zoning report including recommendations prepared by Dufresne-Henry. He indicated that this is the third Linden Street workshop to focus on zoning. The first workshop held on Tuesday, February 5, 2002 focused on traffic circulation. The second held on Tuesday, March 19, 2002 focused on parking. He gave an overview of zoning in town and in the study area. He said the focus of this aspect of the study is toward establishing an overlay district. He introduced Mr. Brovitz.

Mr. Brovitz said the vision phase of the study identified the need for "keeping things the way they are" and "pedestrian safety." He said that in developing the zoning recommendations there has been an effort to avoid changing the underlying zoning. He said the majority of the land is zoned Industrial but used as commercial.

Ms. Sarah Murphy said that there are industrial uses such as the gas station, auto body shop, and lumberyard and car dealership. She said these are not by-right uses in the Business District but they are by-right in the Industrial.

Mr. Brovitz said that the current project permitting process is cumbersome. The overlay would attempt to streamline that process.

Ms. Beth Murphy said the apartment area is zoned as Business. Would that change? Mr. Brovitz responded that the underlying zoning would remain and not be changed.

Mr. Brovitz said the goals of the overlay would be to encourage more infill development and encourage buildings closer to the street.

Ms. Lukas said if that is done where will people park? People will be upset if parking is lost. Mr. Brovitz said that shared parking is the goal.

Mr. Frisardi asked if anything would be made non-conforming. Mr. Brovitz responded in the negative.

Mr. Dennis Di Schino asked if there is any change to FAR and what is the actual FAR. Mr. Brovitz responded that there would be no FAR in the overlay and indicated that he did not know the actual FAR but would find out.

Mr. Sarginson called for more attention to pedestrian amenities along the storefronts rather than concentrating attention at the street line. He said there should be more attention to sheltering customers from the wind, rain and sun along the storefronts.

Mr. Jackson said a goal of the overlay is to provide more flexibility for property owners than now exists and to encourage a wider mix of uses in the buildings and increase in FAR.

Mr. Dennis Di Schino expressed skepticism at a goal of increasing density without adequate parking. He said that in the Lower Falls district there is no requirement for parking.

Mr. Sarginson felt that changing the FAR would be impossible.

Ms. Beth Murphy said the increasing the density of development would not necessarily make the area more pedestrian friendly.

Mr. Robert Di Schino noted that Section 6.6 calls for alternative access to the business area. He wondered how this could be done and felt that in any event it would be quite controversial.

Mr. Sechrest noted that it was stated that the 30-foot setback seemed to preclude certain goals of the study. He wondered whether a 10 to 15 foot setback rather than 30 feet would work. Mr. Brovitz thought it would.

Mr. Meade felt that major benefits could be achieved to improve the area through the proposed streetscape, pedestrian safety, and traffic flow improvements.

Mr. Dennis Di Schino said there should be consideration of the General Residence District and making that district more useful in providing for housing opportunities in the area. Mr. Grignaffini reinforced this point and said that there is a real need for small units with a master bedroom on the first floor for older people who are leaving their houses and want smaller units in town. He said that the General Residence district is perfect for this because folks can walk to the grocery store, the pharmacy and to the train station.

Mr. Brown noted that the zoning must be drafted to protect the town from unforeseen land use changes. He mentioned the 27 Washington Street situation. He said the land is privately owned and as such the town really has little control except through zoning. He said everyone loves Diehl's but as a private business the town cannot mandate that it stay in business forever. He referenced the Community Playhouse Theater situation by comparison. He also mentioned the difference between retail that serves the town and regional retail.

Mr. Robert Di Schino said that the local business that draws from the region is not bad. Businesses need customers from other towns to be successful.

Mr. Sechrest said that a "big box" would not locate on Linden Street.

There being no further comments Mr. Brovitz turned the meeting back to Mr. Hansen. He mentioned that the consultants and he are meeting tomorrow with representatives of the MBTA to discuss issue of access across the tracks. A suggested meeting date for the draft study presentation to the Board of Selectmen and the Linden Street Project Committee of June 11, was rescheduled to Tuesday June 18, 2002 at 7:30pm. in the Selectmen's Meeting room, to avoid a conflict with the Scholarship Awards Ceremony function occurring on Tuesday night June 11<sup>th</sup>.

All were thanked for their attendance.

Session concluded at 9:00 pm.

Richard H. Brown  
Planning Director

## Appendix 4: Project Participants

### Linden Street Workshop Attendees

Ralph DiGiandomencio - F. Diehl & Son, Inc.  
Sarah A. Murphy - F. Diehl & Son, Inc., Linden Street Association of Businesses, Resident  
Beth A. Murphy - F. Diehl & Son, Inc., Resident  
James W. Devaney - Diehl Oil, Resident  
Jeanne Lukas - Wellesley Chamber of Commerce  
Keith Marden - Captain Marden's owner, Resident  
Valerie Martin - Captain Marden's - Business Manager  
Dennis Dischino - Area Resident, Building Owner & Manager  
Bob Annand, Sr. - Roche Bros - President  
Dan Lanzillo - Roche Bros - Store Manager  
Jon Erickson - Roche Bros.  
Jim Sperber - Roche Bros.  
Ronald Labadini - Building Owner - One Hollis Street, Resident  
Joe Grignaffini - Builder / Owner / Resident  
Joe Perdoni - Building Owner - 139 Linden Street, Resident  
Rock Perdoni - Building Owner - 139 Linden Street  
Eugenio Perdoni - Building Owner - 139 Linden Street, Business owner, Resident  
Charles Boiardi - Building Owner - 151 Linden Street  
Bill Cullin- Building Owner - 151 Linden Street  
Dean Behremd - Builder/Developer/ Resident  
Greg LeBrun - The Linden Store  
Lou Petrucci - Linden Texaco  
Willie Williams - Wellesley Volkswagen  
Michael F. Collins - Wellesley Volkswagen  
Michelle & Dennis Shea - Fitness Club for Women  
Sharlene Harris & Jack Dean - Middlesex Savings Bank  
Walter Henderson - Cambridgeport Bank  
Edwin Silver - Wellesley Co-operative Bank  
Thomas Fontaine - Wellesley Co-operative Bank  
Jack Hester – Area Resident  
Janie Batista – Area Resident  
Bill Hutchinson – Area Resident  
Laura & John Colburn - Residents  
Lawrence Damon - Linden Printing, Resident  
Paul Cremonini – Area Resident  
Pam Kubbins & Paul Criswell – Area Residents  
Tim Hill and Jan Schwaner – Area Residents  
Marjorie Paltrineri – Area Resident  
Lewis Collins – Area Resident & Trails Committee  
Robert White – Resident, Advisory Committee & Trails Committee  
Shannon Scarlett – Area Resident  
Arshag Merguerian – Area Resident and Design Review Board member  
Jim Fitzgerald - MAPC Transportation Planner  
Mark Boyle - MBTA Real Estate Department Director  
Mike Vignale – BETA Engineering

## **Town Officials**

Mike Pakstis - Department of Public Works Director  
Tom Schnorr - Wellesley Housing Dev. Corp., Resident  
Steve Fader - Department of Public Works Town Engineer  
Kathy Macdonald - Wellesley Housing Dev. Corp., Resident  
Judy Curby - Department of Public Works (DPW)  
Dona M. Kemp - Wellesley Housing Dev. Corp., Resident  
Dick Joyce - Municipal Light and Power (MLP)  
Rusty Kellogg - Wellesley Housing Dev. Corp., Resident  
Terry Cunningham - Wellesley Police Chief  
Bob Kenney - Wellesley Housing Dev. Corp., Resident  
William Brooks - Wellesley Police Deputy Chief  
Irene Flint – Advisory Committee - Planning Board Rep  
Susan Littlefield - Wellesley Public Schools  
Lu Awdeh – Advisory Committee - previous Planning Board Rep  
Vince McCarthy - Council on Aging  
Thomas Frisardi – Planning Board Chair  
Arnold Wakelin – Board of Selectmen  
James Meade – Planning Board Vice Chair  
Christopher Clark – Board of Selectmen  
Rose Mary Donahue – Planning Board Secretary  
Peter W. Gubellini – Board of Selectmen, Chair  
Patricia Grandieri - Planning Board Project Liaison  
David Himmelberger - Board of Selectmen, Vice Chair  
Bob Sechrest – Planning Board member  
Harriet Warshaw - Board of Selectmen, Previous Chair  
Bob Dischino –resident & previous Planning Board member  
Laurence D. Shind – Board of Selectmen  
Sue Wright – Planning Board Associate Member  
Vincent Juliani, Jr. – Board of Selectmen  
Rick Brown – Planning Director  
Janet Bowser - Natural Resources Commission (NRC) Director  
Meghan Conlon – Town Planner  
Kathy Mullaney - Permanent Building Committee (PBC)  
Lance D. Hansen – Comprehensive Town Planner

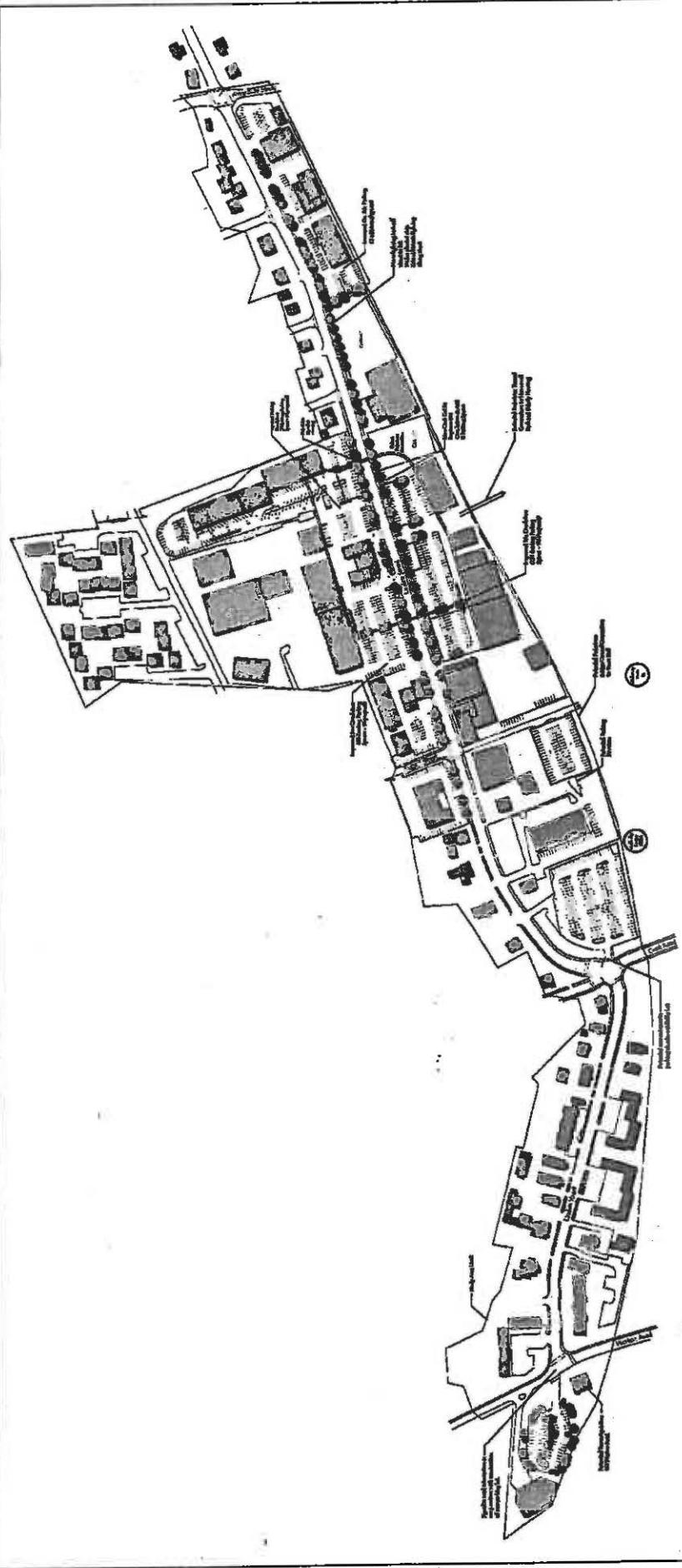
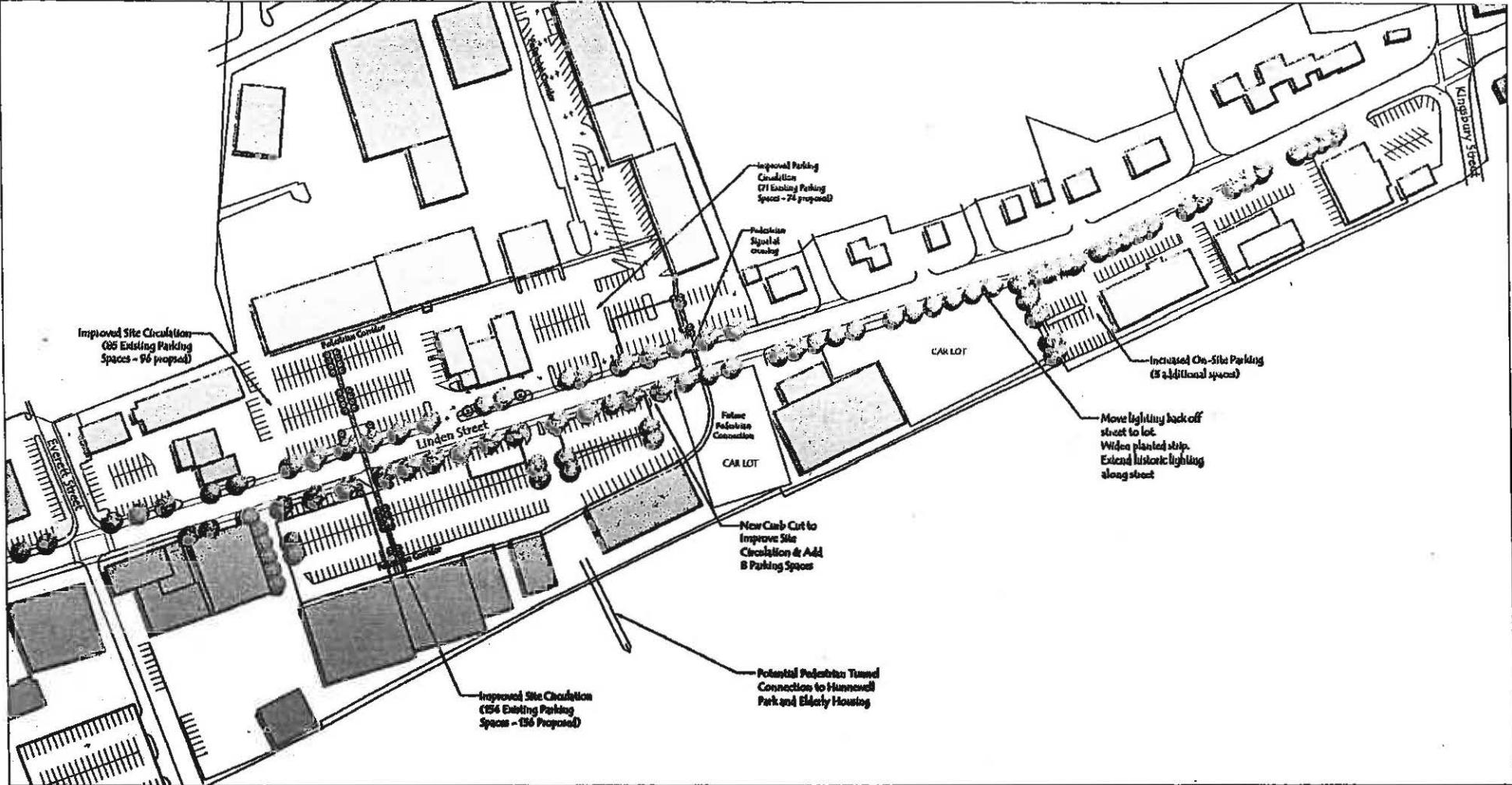


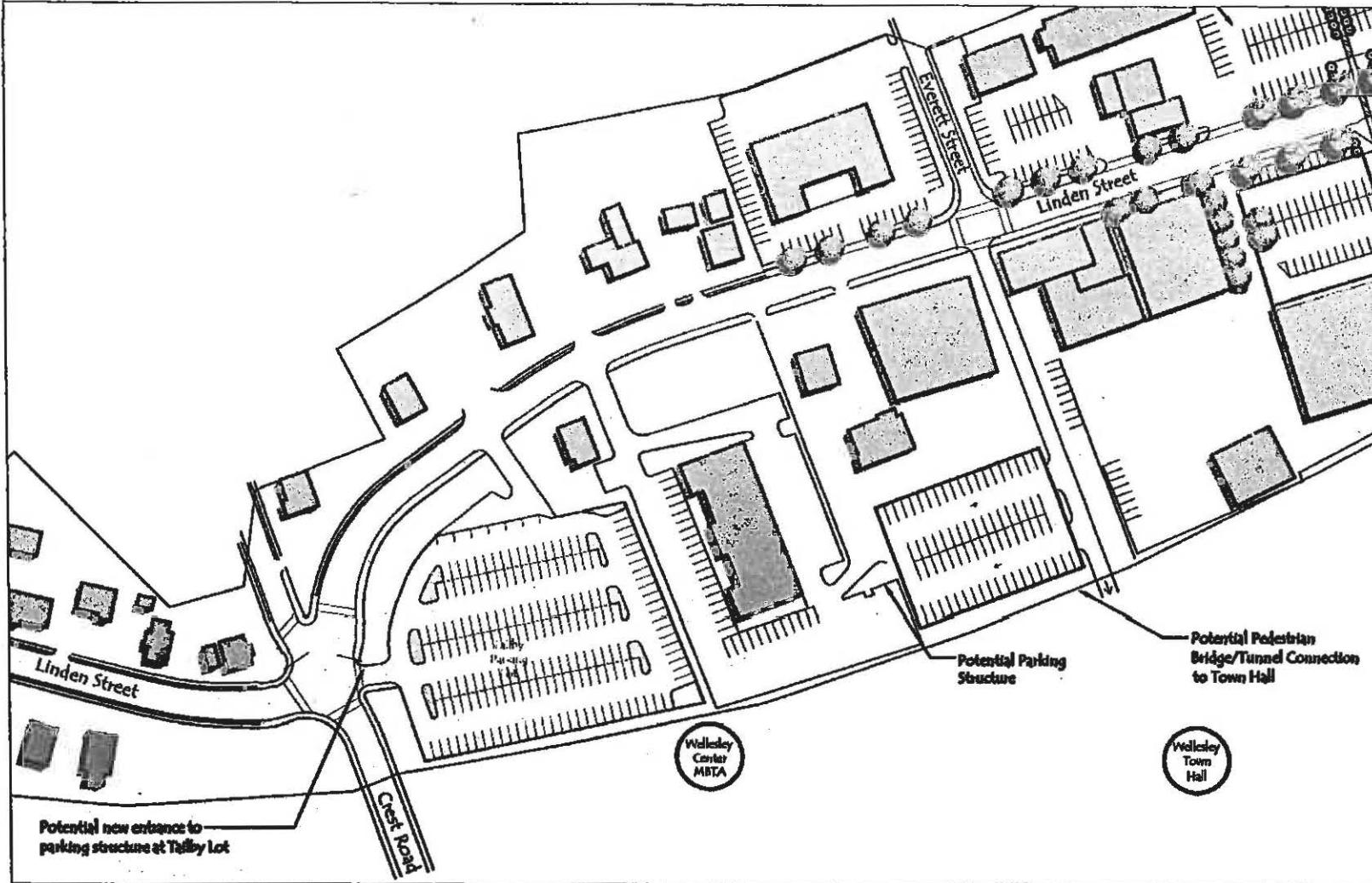
Figure 6: Linden Street Corridor Improvement Plan  
Wellesley, Massachusetts





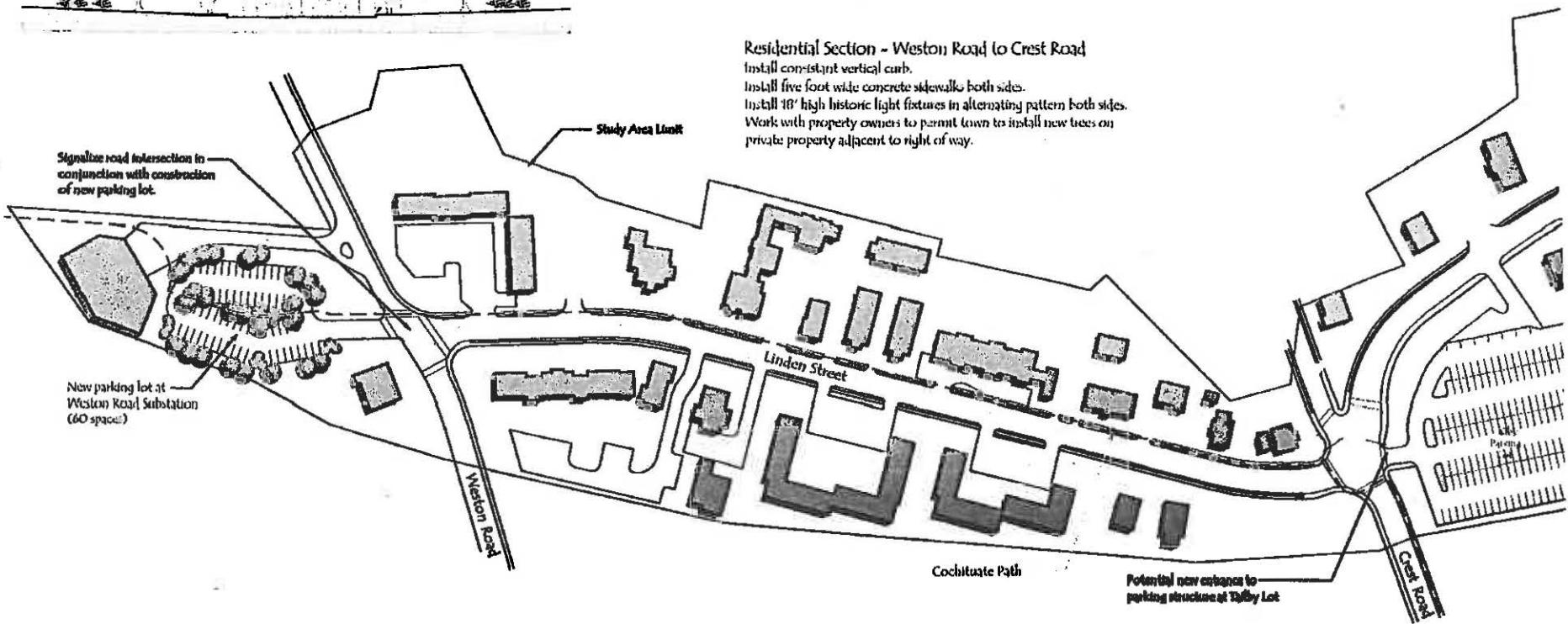
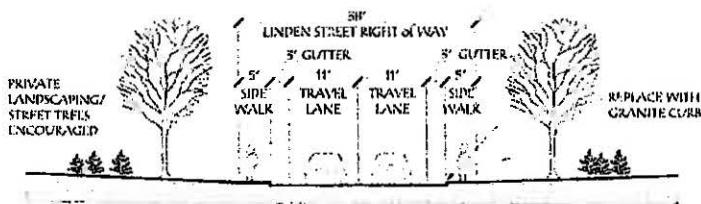
**Figure 6A: Linden Street Corridor Improvement Plan - Kingsbury Street to Everett Street**  
**Wellesley, Massachusetts**





**Figure 6B: Linden Street Corridor Improvement Plan - Everett Street to Crest Road**  
**Wellesley, Massachusetts**





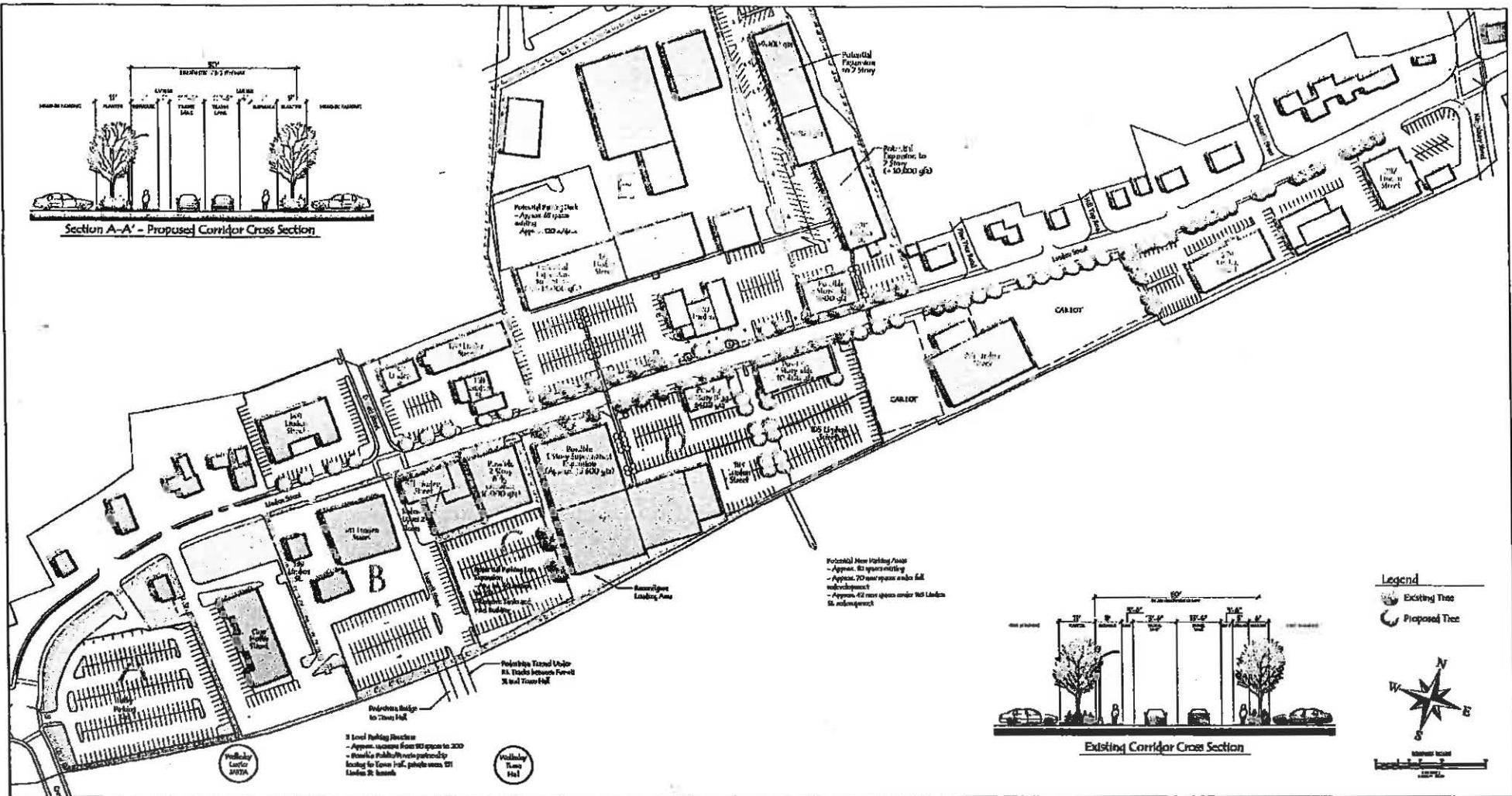
**Figure 6C: Linden Street Corridor Improvement Plan - Crest Road to Weston Road**

Wellesley, Massachusetts



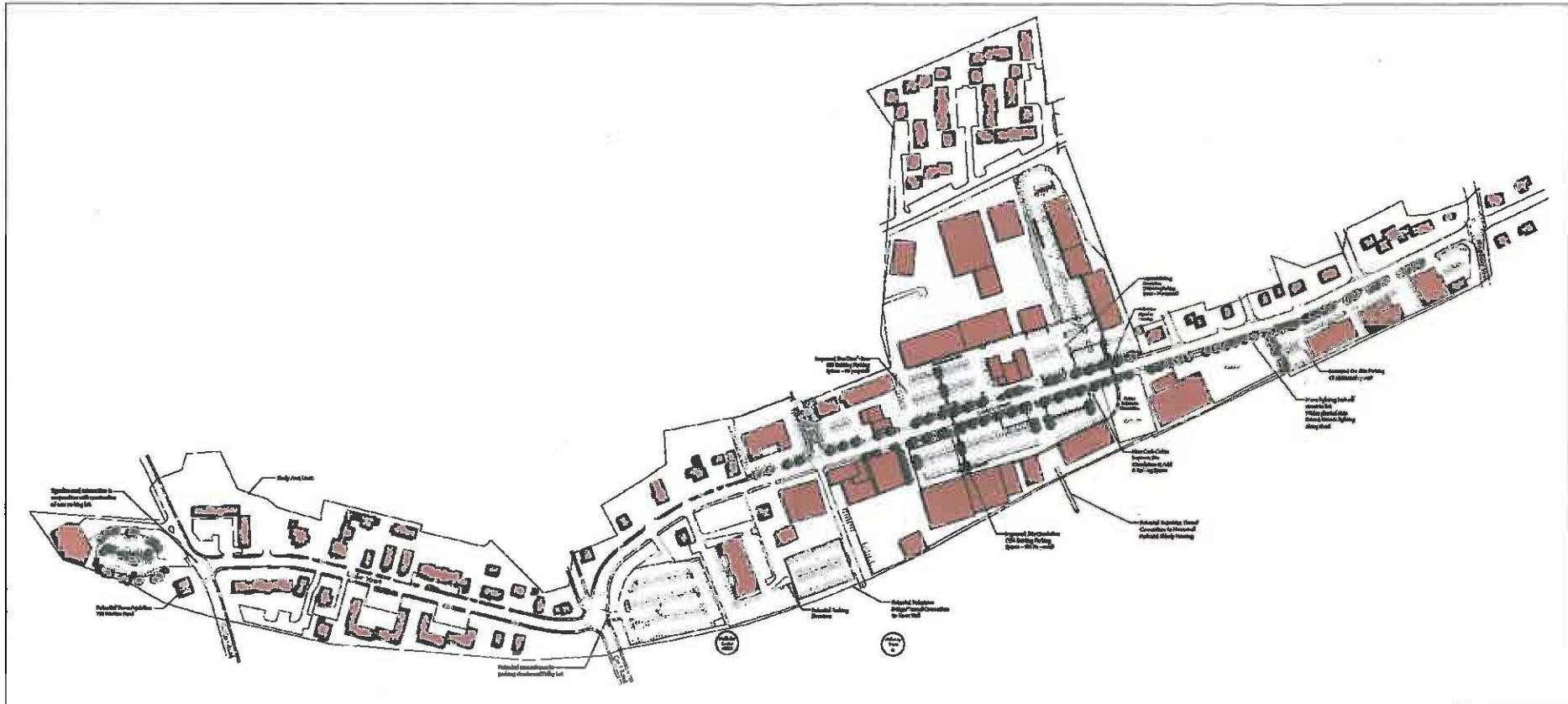
TABLE 8: LINDEN STREET CORRIDOR 10-YEAR IMPLEMENTATION &amp; ACTION PLAN

STRATEGIES & RECOMMENDATIONS	TARGET YEAR	ACTIONS & DESCRIPTIONS	LEADERS & PARTNERS	PROJECTED COSTS	POTENTIAL FUNDING	PRIORITY
<b>TRAFFIC CIRCULATION, PARKING &amp; PEDESTRIAN IMPROVEMENTS</b>						
Install new traffic signals at Crest Street and Everett Street	2002	Project was completed in the summer of 2002	Town	N/A	Town and State	Completed
Weston Road Parking Improvement Plan	2005	New 60 car parking lot; intersection signalization	Town	\$400,000	Town	3
Tailby Parking Lot Improvement Plan – Alternative 1	2005	New parking deck and lighting for a total of 406 spaces	Town and MBTA	\$2,800,000	Town and MBTA	2
Tailby Parking Lot Improvement Plan – Alternative 2	2005	New parking deck, lighting and 18 housing units for a total of 343 spaces	Town, MBTA and Wellesley Housing Development Corp.	\$2,000,000 + housing cost	Town, MBTA and Housing Authority	2
Tailby Parking Lot Planning	2003	Establish Planning Task Force	Board of Selectmen/Planning Board	NA	NA	1
Construct Internal Pedestrian Connector	2004	Paving and landscaping	Business owners	\$ 150,000	Private	3
Create partnership between town and property owners to design and build parking structure at 141 Linden Street	2008	New parking structure with approximately 300 spaces serving existing and future business demand and Town Hall	Town and property owners	\$ 3,000,000	Town and property owners	4
Work with MBTA and property owners to construct a railroad underpass	2008	New pedestrian underpass behind Wellesley Volkswagen or 151 Linden St. in combination with new parking structure	Town and MBTA	\$ 550,000	Town	5
Access management and curbcut consolidation	ongoing	Work with property owners to eliminate/consolidate selected curb cuts and configure parking areas to provide additional parking and better circulation	Town and property owners	\$ 150,000	Town and property owners	1
<b>STREETSCAPE IMPROVEMENTS &amp; GATEWAY IMPROVEMENTS</b>						
Linden Street Corridor Improvement Plan – Phase I	2004	Everett Street to Kingsbury Street	Town		Town	1
Sidewalks		Concrete with brick accent		\$200,000		
Pedestrian Signals		1 pedestrian flashing signal		\$ 125,000		
Landscaping		planting and irrigation		\$ 150,000		
Lighting		20 light fixtures @ \$5000 each		\$ 100,000		
Pedestrian Node		Benches, paving, plantings, drinking fountain		\$ 40,000		
Linden Street Corridor Improvement Plan – Phase II	2006	Weston Street to Everett Street	Town		Town	3
Sidewalks		Concrete with brick accent		\$150,000		
Landscaping		planting and irrigation		\$ 50,000		
Lighting		16 light fixtures @ \$5,000 each		\$ 80,000		
Install Gateway Signage	2004	Attractive, high quality directional signage	Town and business owners	\$ 40,000	Town and property owners	2
<b>DESIGN GUIDELINES, ZONING &amp; DEVELOPMENT STANDARDS</b>						
Amend Zoning Ordinance for Linden Street Districts	2003	Amend ordinances for site plan review, parking, landscaping and signage regulations	Town and business owners	N/A	N/A	1
Adopt new traditional development overlay district ordinance	2003	New flexible ordinance facilitating high quality, traditional design and mixed use development in the corridor	Town and business owners	N/A	N/A	1
<b>POLICIES &amp; PROGRAMS</b>						
Adopt a resident sticker program for parking at Tailby Lot	2003	Allocate percentage of parking spaces for town residents	Town	N/A	N/A	1
Develop a permit program for parking at Weston Road Lot	2005	For Central Square business owners and employees	Town	N/A	N/A	3
Support the Linden Street Merchants Association	on-going	Form a partnership between the Town and Linden Street Business Committee to promote the corridor and encourage high quality public and private development	Town and business owners	N/A	N/A	1



**Figure 10: Ten Year Potential Development and  
Redevelopment Scenarios in Linden Street Corridor**  
**Wellesley, Massachusetts**



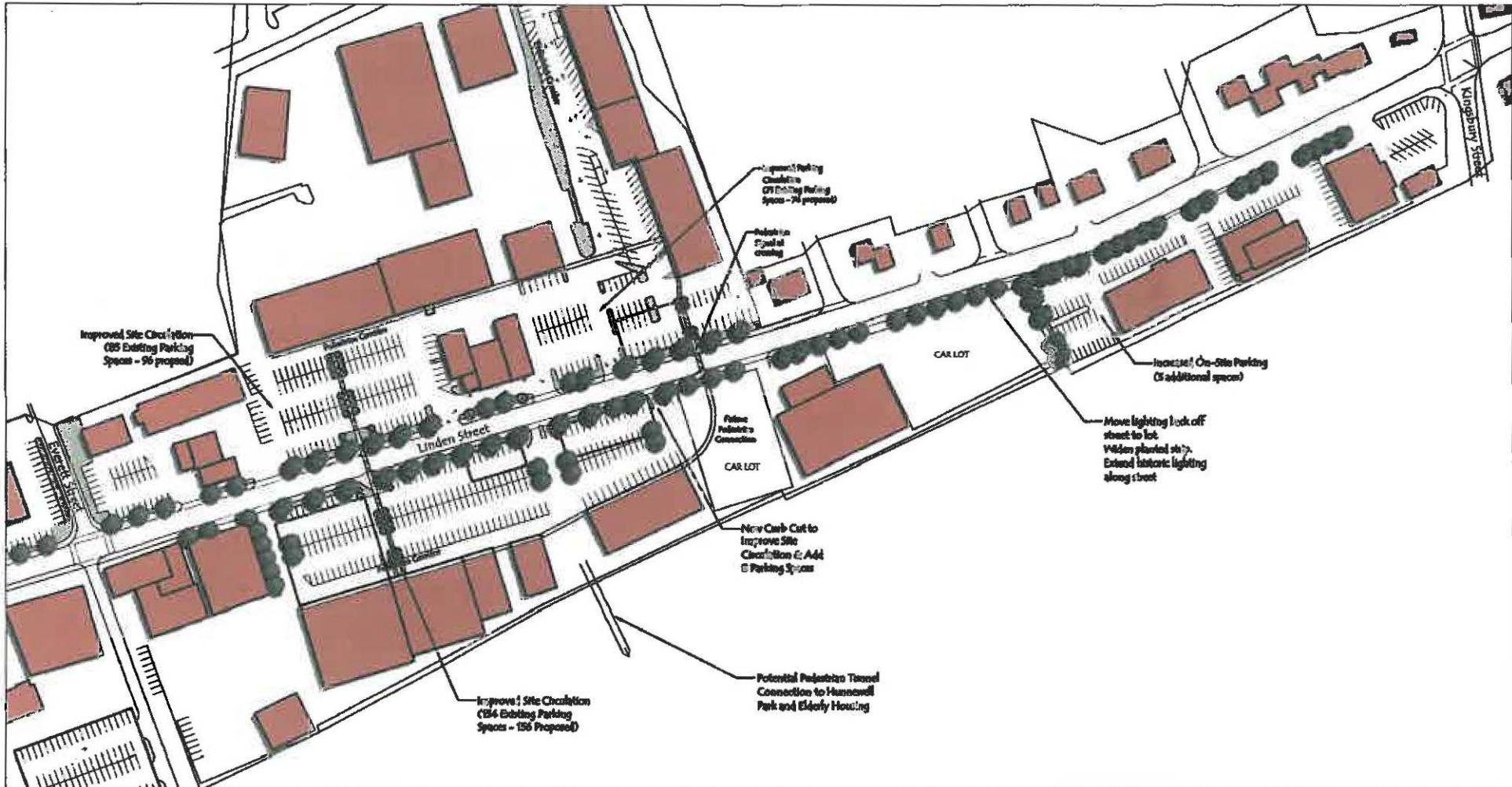


## Figure 6: Linden Street Corridor Improvement Plan

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Wellesley, Massachusetts

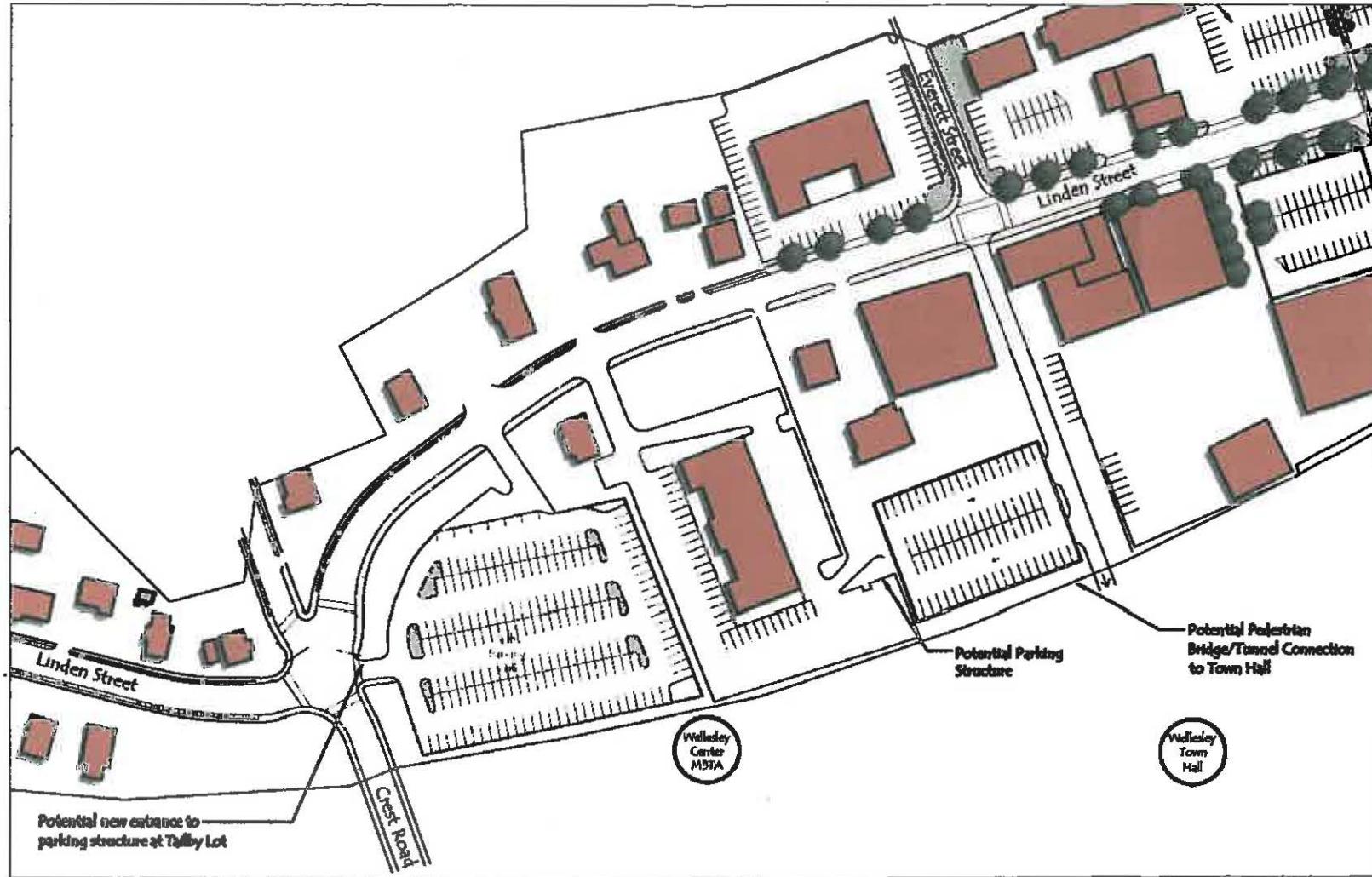




**Figure 6A: Linden Street Corridor Improvement Plan - Kingsbury Street to Everett Street**

Wellesley, Massachusetts





**Figure 6B: Linden Street Corridor Improvement  
Plan - Everett Street to Crest Road  
Wellesley, Massachusetts**



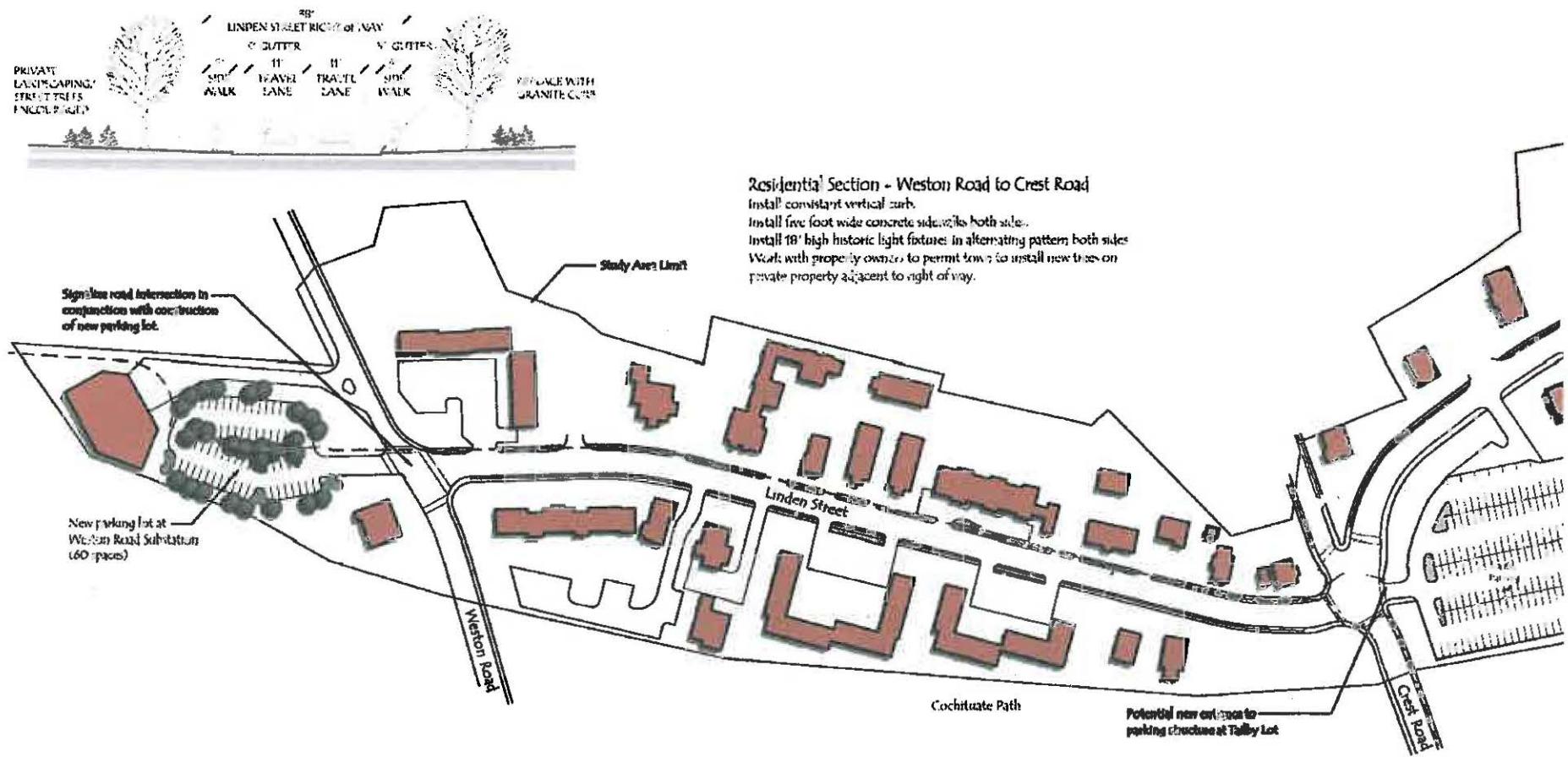


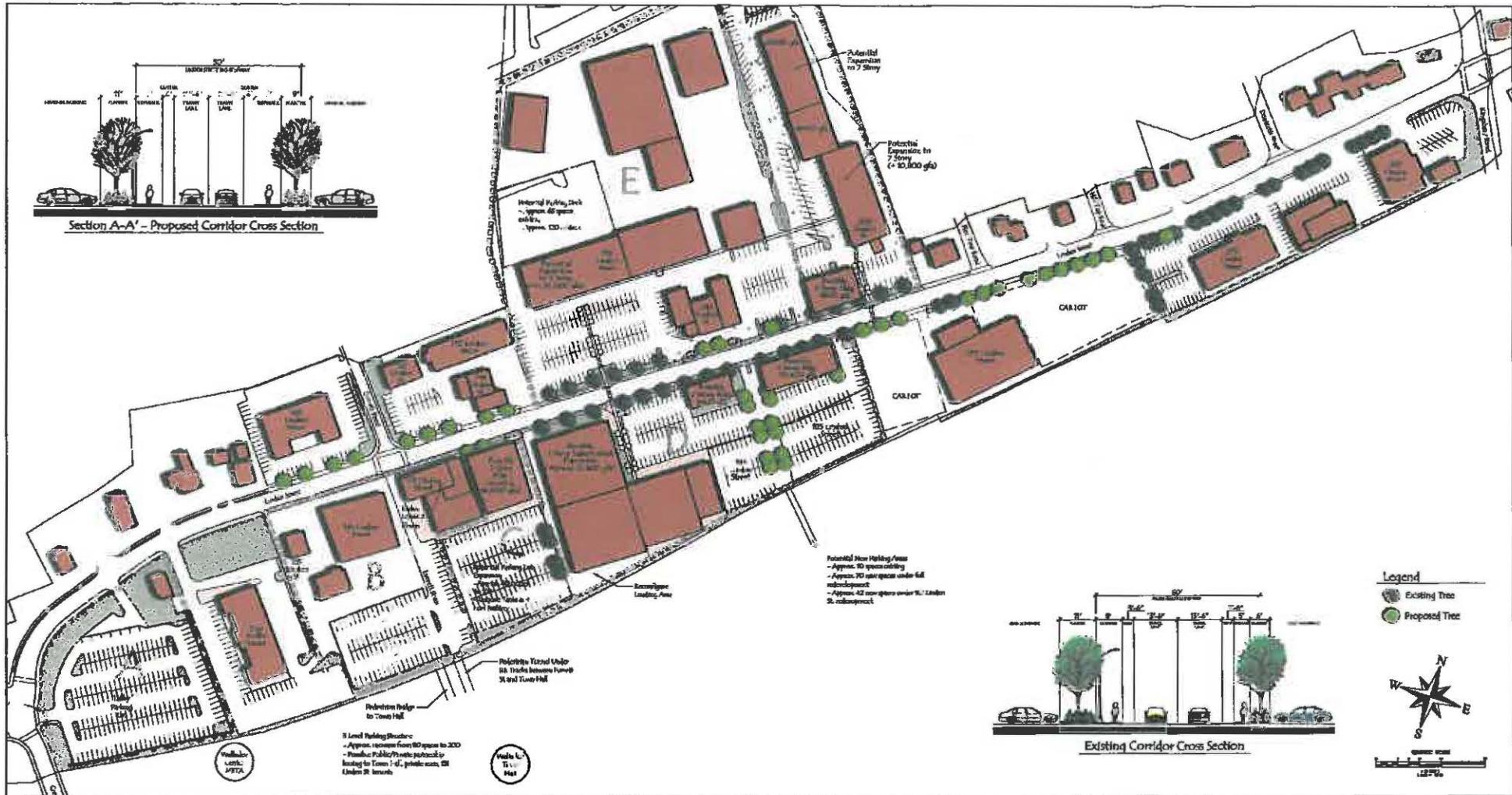
Figure 6C: Linden Street Corridor Improvement Plan - Crest Road to Weston Road

Wellesley, Massachusetts



TABLE 8: LINDEN STREET CORRIDOR 10-YEAR IMPLEMENTATION &amp; ACTION PLAN

STRATEGIES & RECOMMENDATIONS	TARGET YEAR	ACTIONS & DESCRIPTIONS	LEADERS & PARTNERS	PROJECTED COSTS	POTENTIAL FUNDING	PRIORITY
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## Figure 10: Ten Year Potential Development and Redevelopment Scenarios in Linden Street Corridor Wellesley, Massachusetts

